



**ROAD
DEPARTMENT**

ROAD CAPITAL IMPROVEMENT PLAN FISCAL YEARS 2024 — 2028



MAY 15, 2023



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ROAD CAPITAL IMPROVEMENT PLAN FISCAL YEARS 2024 - 2028

Date: May 15, 2023

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TABLE OF CONTENTS

Executive Summary.....	1
Introduction.....	1
Project Selection.....	3
Funding.....	4
CIP Organization.....	6
Appendix A – FY 2024-2028 CIP Summary	
Appendix B - FY 2024-2028 CIP Program Maps	
Appendix C – Project Information Sheets and Location Maps	



EXECUTIVE SUMMARY

This report summarizes the Deschutes County Road Department’s five-year Road Capital Improvement Plan (CIP) for Fiscal Year 2024 through Fiscal Year 2028. The Department has identified \$77,315,530 in total project expenditures for the five-year period. The projects outlined in the CIP include intersection and corridor improvements, major pavement preservation, bridge replacement and rehabilitation, and other project types to modernize, maintain, and increase the safety of the County road system. This report includes the summary, justification, scope of work, budget, anticipated schedule, and location map for each project.

INTRODUCTION

Deschutes County Road Department is responsible for the improvement, operation, and maintenance of the 926-mile County road system. Pursuant to the Deschutes County Transportation System Plan and Federal Highway Administration standards, County roads are grouped under one of three main functional classifications:

- Arterial
 - Links cities, larger towns, and other major traffic generators, providing interregional service.
 - Spaced at distances so that all developed areas are within reasonable distance of an arterial highway.
 - Provides service to corridors with trip length and travel density greater than that predominately served by rural collector or local systems.
 - Secondary route for movement of goods and services.

- Collector
 - Distributes trips from arterials to their final destination, and conversely, collects traffic from local streets and channels it onto arterials.
 - Spaced at intervals to collect traffic from local roads and provide all developed areas a reasonable distance from a collector road.
 - Provides service to the remaining smaller communities.
 - Links locally important traffic generators with rural destinations.

- Local
 - Primarily provides access to adjacent land/properties.
 - Accommodates travel over short distances as compared to arterials and collectors.
 - Provides access to adjacent land and access to higher classified roads.
 - Carries less than 1,500 vehicles per day.

Much of the County’s arterial and collector road network was established prior to the 1940s, while much of the County’s local road network was established through private development after the 1940s. With



the population and tourism growth in Deschutes County in recent years, the need to improve mobility and safety on the County Road systems has become increasingly crucial.

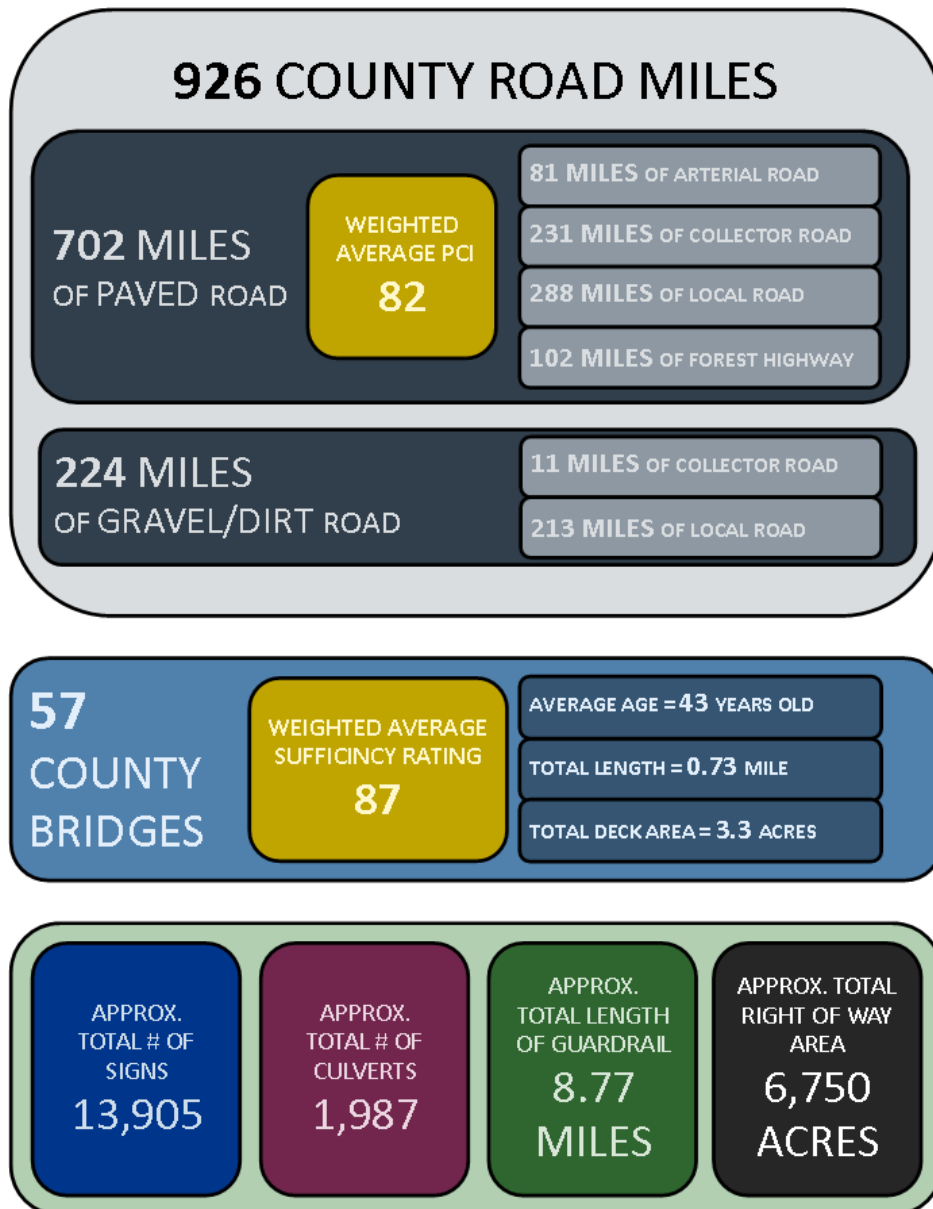


FIGURE 1 – DESCHUTES COUNTY ROAD SYSTEM AT A GLANCE

This report has been developed to:

- Identify the Department’s Capital Improvement Plan for Fiscal Years 2024 through 2028.
- Guide the annual development of the Department’s Road CIP Fund budget.
- Identify funding sources and funding requirements for projects.
- Assist the Department Director and Department managers with the scheduling of staff and resources.



PROJECT SELECTION

Projects included in the Department's Capital Improvement Plan are selected from the following primary sources:

- **Transportation System Plan**

The County's Transportation System Plan (TSP) was adopted as part of the County Comprehensive Plan in compliance with Statewide Planning Goal 12. The Transportation System Plan includes an inventory of the County's transportation infrastructure and identifies long-term goals and project needs determined through an extensive public input process. The current TSP covers the 20-year period from 2010 to 2030. Projects identified in the TSP have been prioritized as High, Medium, and Low priority.

The County is currently in the process of updating the TSP for years 2020 through 2040; this update is anticipated to be finalized in Calendar Year 2023.

- **Asset Management Programs**

The Department implements various asset management programs to efficiently manage over \$550 million worth of County road infrastructure. These asset management programs generally include a database of all the County's road assets and their condition that is populated through routine inspections conducted by Department staff or contractors. Projects are identified within the Department's asset management programs based on asset conditions, then are prioritized based on safety concerns, lifecycle cost, functional classification, or other criteria.

Road asset management programs implemented by the Department include:

- *Pavement Management System* – The Department manages its pavement assets using the StreetSaver web-based software application. The system includes the routine inspection of sample units of every paved roadway section in the County road network. Inspection data is used to calculate a pavement condition index (PCI) for each unit, in turn calculating a weighted average PCI for each road section, each road, and the County road network as a whole.
 - *Bridge Management System* – The Department manages its bridge assets using the PONTIS web-based application through ODOT. The system includes routine element inspections of all bridges. Inspection data, combined with load rating reports and other bridge data, is used to calculate a sufficiency rating for each bridge and provide maintenance and improvement recommendations.
 - *General Asset Management* – All other road appurtenant assets, including signs, guardrails, culverts, storm sewers, cattle guards, illumination, traffic signals, roadside shoulders, and other features, are managed through PubWorks, an enterprise software that the Department operates for internal cost accounting and general asset management.
- **Special Studies and Planning Efforts**
The Department conducts special studies when needed to evaluate safety, operations, or other concerns for hotspot locations, corridors, or systemically. The need for these studies generally



arises as a result of changes to traffic volumes or patterns in a particular area or implementation of new state or federal standards.

- State and Federal Funding Programs

The Department actively pursues funding through several state and federal programs. Recurring funding programs that are dedicated to particular project types include:

- *Surface Transportation Block Grant Program* (ODOT) – Annual local agency allocation of federal Surface Transportation Program that provides funding for any State Highway Fund-eligible projects or procurements.
- *Federal Lands Access Program* (FHWA) – Provides funding for projects on roads that provide access to federal land high use recreation sites or economic generators.
- *Local Bridge Program* (ODOT) – Provides funding for Oregon local agency bridge rehabilitation or replacement projects.
- *All Roads Transportation Safety Program* (ODOT) – Provides funding for hotspot and systemic safety improvements on Oregon public roads.
- *Safe Routes to Schools* (ODOT) – Provides funding for projects that enhance safe multimodal transportation to Oregon schools.

FUNDING

Contracted public improvement and major maintenance projects are budgeted annually in the Road CIP Fund (Fund 465) budget. Road CIP Fund revenue comes from the following primary sources:

- Transfer from Road Fund – The Road Fund is the Department’s operating budget. The Department implements a “trickle-down” approach to budgeting for the Road Fund, whereby baseline operations and maintenance of the County road system are prioritized, and remaining revenues are transfer to the Road CIP fund. The primary source of the Road Department’s revenue is the allocation received from the State Highway Fund (SHF). The State Highway Fund is comprised of:
 - Fuel Tax (46%)
 - Motor-Carrier Fees (Freight), weight-mile tax (32%)
 - DMV Fees (license, registration, etc), (22%)

Approximately 30% of the annual State Highway Fund revenue is allotted to Oregon counties, with each county receiving an amount based on the number of registered vehicles in the county. In June of 2017, the Oregon Legislature passed a transportation funding package via HB 2017 which provided a 7-year phase-in of a 10-cent state fuel tax increase, in addition to other funding mechanisms. The additional funding delivered by this legislation has provided a substantial revenue infusion to the Road CIP Fund. Other Road Fund revenue sources include federal payments such as Payment In Lieu of Taxes (PILT), federal lands timber receipts, and the Secure Rural Schools Act payments.

- System Development Charges – Private development in unincorporated Deschutes County is charged a one-time assessment based on the number of P.M. peak-hour trips the development is anticipated to generate. These system development charges are used to fund projects identified



in the TSP based on the methodology instituted with the County’s system development charge resolution (Resolution No. 2013-020).

- State/Federal Grants – Upon award of funding through state and federal programs described above.

	FY 2024	FY 2025	FY 2026	FY 2027	FY 2028
<u>ROAD FUND (325) REVENUE</u>					
BEGIN FUND BALANCE	\$ 5,521,251	\$ 2,890,389	\$ 2,977,101	\$ 3,066,414	\$ 3,158,406
STATE HWY FUND	\$ 20,648,483	\$ 22,000,000	\$ 22,500,000	\$ 23,000,000	\$ 23,500,000
FOREST RECEIPTS	\$ 689,703	\$ 689,703	\$ 689,703	\$ 689,703	\$ 689,703
PILT	\$ 2,240,000	\$ 2,240,000	\$ 2,240,000	\$ 2,240,000	\$ 2,240,000
PARTNER AGENCY REVENUE (1% INCREASE/YR)	\$ 1,156,342	\$ 1,167,905	\$ 1,179,584	\$ 1,191,380	\$ 1,203,294
EQUIPMENT/MATERIAL SALES (3% INCREASE/YR)	\$ 614,500	\$ 632,935	\$ 651,923	\$ 671,481	\$ 691,625
INTERFUND PAYMENTS C	\$ 1,450,015	\$ 1,493,515	\$ 1,538,321	\$ 1,584,471	\$ 1,632,005
OTHER (1% INCREASE/YEAR)	\$ 128,808	\$ 130,096	\$ 131,397	\$ 132,711	\$ 134,038
INTEREST (0.5%)	\$ 139,031	\$ 156,223	\$ 159,540	\$ 162,881	\$ 166,245
TOTAL ROAD FUND (325) REVENUE	\$ 32,588,133	\$ 31,400,767	\$ 32,067,569	\$ 32,739,040	\$ 33,415,317
<u>ROAD FUND (325) EXPENDITURE</u>					
PERSONNEL (3% INCREASE/YEAR)	\$ 8,284,451	\$ 8,532,985	\$ 8,788,974	\$ 9,052,643	\$ 9,324,223
MATS/SERV (3% INCREASE/YEAR)	\$ 8,595,033	\$ 8,852,884	\$ 9,118,471	\$ 9,392,025	\$ 9,673,785
CAPITAL OUTLAY (3% INCREASE/YEAR)	\$ 118,260	\$ 121,808	\$ 125,462	\$ 129,226	\$ 133,103
TRANSFER TO ROAD BLDG/EQUIP CIP (330)	\$ 2,700,000	\$ 2,500,000	\$ 2,500,000	\$ 3,000,000	\$ 3,000,000
TRANSFER TO ROAD CIP (465)	\$ 10,000,000	\$ 8,415,990	\$ 8,468,249	\$ 8,006,740	\$ 8,031,048
CONTINGENCY (3% INCREASE/YEAR)	\$ 2,890,389	\$ 2,977,101	\$ 3,066,414	\$ 3,158,406	\$ 3,253,158
TOTAL ROAD FUND (325) EXPENDITURE	\$ 32,588,133	\$ 31,400,767	\$ 32,067,569	\$ 32,739,040	\$ 33,415,317
<u>SDC FUND (336) REVENUE</u>					
BEGIN FUND BALANCE	\$ 1,484,022	\$ 526,622	\$ 154,721	\$ 73,243	\$ 93,243
SDC (6% INCREASE/YEAR)	\$ 1,500,000	\$ 1,590,000	\$ 1,685,400	\$ 1,786,524	\$ 1,893,715
INTEREST/OTHER (1.8%)	\$ 42,600	\$ 38,099	\$ 33,122	\$ 33,476	\$ 35,765
TOTAL SDC FUND (336) REVENUE	\$ 3,026,622	\$ 2,154,721	\$ 1,873,243	\$ 1,893,243	\$ 2,022,724
<u>SDC FUND (336) EXPENDITURE</u>					
TRANSFER TO ROAD CIP (465)	\$ 2,500,000	\$ 2,000,000	\$ 1,800,000	\$ 1,800,000	\$ 2,000,000
CONTINGENCY	\$ 526,622	\$ 154,721	\$ 73,243	\$ 93,243	\$ 22,724
TOTAL SDC FUND (336) EXPENDITURE	\$ 3,026,622	\$ 2,154,721	\$ 1,873,243	\$ 1,893,243	\$ 2,022,724
<u>ROAD CIP FUND (465) REVENUE</u>					
BEGIN FUND BALANCE	\$ 19,012,380	\$ 9,624,306	\$ 10,431,764	\$ 4,227,274	\$ 4,689,811
TRANSFER FROM ROAD FUND (325)	\$ 10,000,000	\$ 8,415,990	\$ 8,468,249	\$ 8,006,740	\$ 8,031,048
TRANSFER FROM SDC FUND (325)	\$ 2,500,000	\$ 2,000,000	\$ 1,800,000	\$ 1,800,000	\$ 2,000,000
ODOT STBG (FEDERAL HWY TRUST FUND)	\$ -	\$ 980,000	\$ 1,009,400	\$ 1,039,682	\$ 1,070,872
OTHER GRANTS	\$ 1,704,116	\$ 1,818,500	\$ 500,000	\$ 500,000	\$ 500,000
INTEREST (1%)	\$ 475,310	\$ 124,563	\$ 217,094	\$ 150,737	\$ 157,917
TOTAL ROAD CIP (465) REVENUE	\$ 33,691,806	\$ 22,963,359	\$ 22,426,507	\$ 15,724,433	\$ 16,449,649
<u>ROAD CIP FUND (465) EXPENDITURE</u>					
CAPITAL OUTLAY	\$ 23,934,730	\$ 12,408,700	\$ 18,087,100	\$ 10,956,000	\$ 11,929,000
MATERIALS & SERVICES (0.5%)	\$ 132,770	\$ 122,895	\$ 112,133	\$ 78,622	\$ 82,248
CONTINGENCY	\$ 9,624,306	\$ 10,431,764	\$ 4,227,274	\$ 4,689,811	\$ 4,438,400
TOTAL ROAD CIP FUND (465) EXPENDITURE	\$ 33,691,806	\$ 22,963,359	\$ 22,426,507	\$ 15,724,433	\$ 16,449,649

TABLE 1 – FY 2024-2028 COUNTY ROAD REVENUE AND EXPENDITURES PROJECTIONS

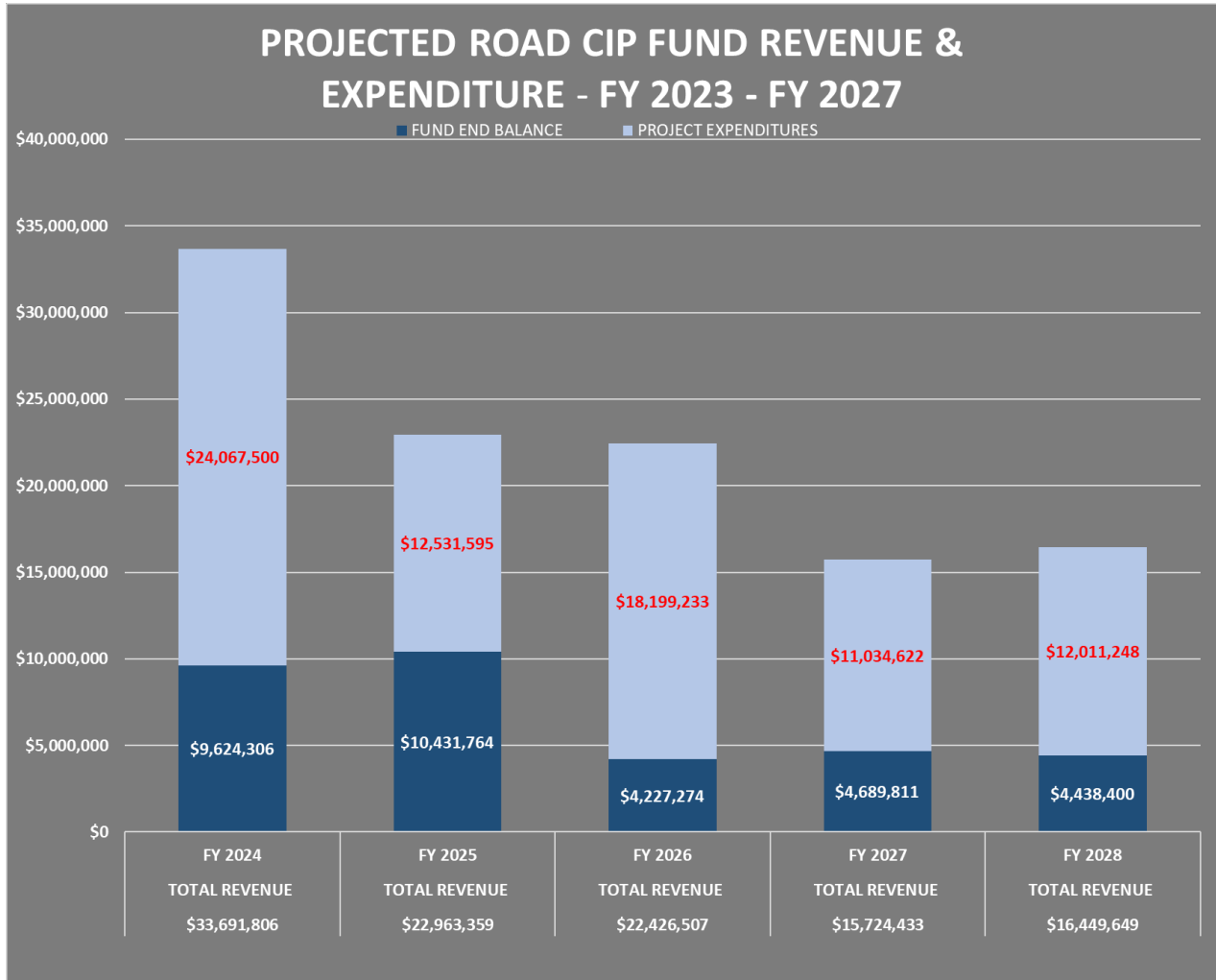


FIGURE 2 – FY 2023-2027 ROAD CIP FUND REVENUE AND EXPENDITURES PROJECTIONS

CIP ORGANIZATION

Projects prioritized and selected from the above-listed sources are programmed into the five-year CIP and scheduled based on available revenue and Department project delivery capacity. Projects are organized in the CIP under four programs:

TRANSPORTATION SYSTEM

Includes major road improvement projects identified in TSP, special studies, or funding agreements. Also includes County contributions to ODOT state highway projects.

PAVEMENT PRESERVATION

Includes contracted pavement preservation projects identified in the Department’s pavement management system.



TRAFFIC SAFETY

Includes contracted safety improvements, including signage, guardrail, and delineation, to bring assets in compliance with current state or federal standards.

BRIDGE CONSTRUCTION

Includes bridge replacement and rehabilitation projects identified in the Department’s bridge management system.

Appendix “A” of this report includes a summary of the five-year Road Capital Improvement Plan (CIP) for Fiscal Years 2023 through Fiscal Year 2027.

Appendix “B” of this report includes County-wide maps depicting the project locations by program. Traffic Safety Program projects and local road pavement preservation projects are not depicted on these maps, as these projects are systemic and cover multiple corridors, some of which are still to be determined.

Appendix “C” of this report includes individual project information sheets and location maps.

Project begin-construction years are indicated on maps and the header of each project information sheet with a color-coded tab, similar to the one depicted in Figure 3 below.



FIGURE 3 – BEGIN-CONSTRUCTION FISCAL YEAR TAB

APPENDIX A

FY 2024-2028 CIP SUMMARY



ROAD DEPARTMENT

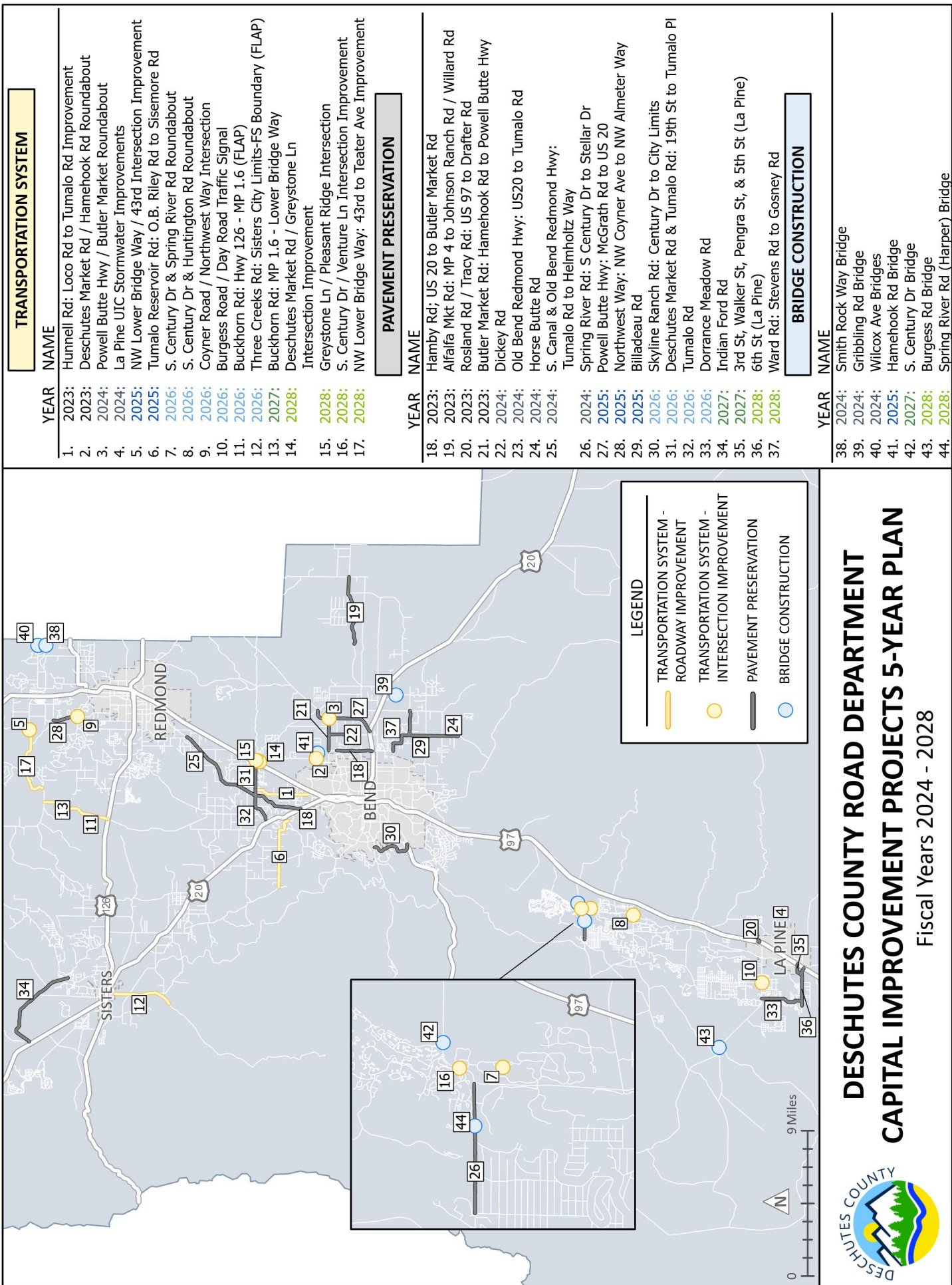
**5-YEAR ROAD CAPITAL IMPROVEMENT PLAN
FY2024-FY2028**

UPDATED: MAY 15, 2023

PROGRAM	PROJECT NAME	FY2024	FY2025	FY2026	FY2027	FY2028	PROJECT TOTALS	
TRANSPORTATION SYSTEM	US 97 - LOWER BRIDGE WAY/TERRERONNE (ODOT)	\$ 5,119,310	\$ -	\$ -	\$ -	\$ -	\$ 5,119,310	
	HUNNELL RD - LOCO RD TO TUMALO RD IMPROVEMENT	\$ 1,569,800	\$ -	\$ -	\$ -	\$ -	\$ 1,569,800	
	DESCHUTES MARKET RD/HAMEHOOK RD ROUNDABOUT	\$ 250,000	\$ -	\$ -	\$ -	\$ -	\$ 250,000	
	POWELL BUTTE HWY/BUTLER MARKET ROUNDABOUT	\$ 2,642,402	\$ -	\$ -	\$ -	\$ -	\$ 2,642,402	
	NW LOWER BRIDGE WAY/NW 43RD ST INTERSECTION IMPROVEMENT	\$ 1,290,000	\$ 1,400,000	\$ -	\$ -	\$ -	\$ 2,690,000	
	TUMALO RESERVOIR RD - O.B. RILEY RD TO SISEMORE RD	\$ 300,000	\$ 1,534,000	\$ 3,489,000	\$ -	\$ -	\$ 5,323,000	
	TERRERONNE WASTEWATER SYSTEM - PHASE 1	\$ 1,000,000	\$ -	\$ -	\$ -	\$ -	\$ 1,000,000	
	LA PINE UIC STORMWATER IMPROVEMENTS	\$ 240,000	\$ -	\$ -	\$ -	\$ -	\$ 240,000	
	S CENTURY DRIVE / SPRING RIVER RD ROUNDABOUT	\$ 177,000	\$ 800,000	\$ 1,877,000	\$ -	\$ -	\$ 2,854,000	
	S CENTURY DR / HUNTINGTON RD ROUNDABOUT	\$ 169,000	\$ 750,000	\$ 1,838,000	\$ -	\$ -	\$ 2,757,000	
	COYNER RD/NORTHWEST WAY INTERSECTION IMPROVEMENT	\$ -	\$ 56,000	\$ 367,000	\$ -	\$ -	\$ 423,000	
	BURGESS RD/DAY RD TRAFFIC SIGNAL	\$ -	\$ 50,000	\$ 746,000	\$ -	\$ -	\$ 796,000	
	BUCHORN RD - MP 1.6 TO LOWER BRIDGE WAY	\$ -	\$ 83,400	\$ 565,700	\$ -	\$ -	\$ 649,100	
	THREE CREEKS RD - SISTERS CITY LIMITS TO FS BOUNDARY (FLAP)	\$ -	\$ 250,000	\$ 980,000	\$ 3,000,000	\$ -	\$ 4,230,000	
	DESCHUTES MARKET RD/GREYSTONE LANE INTERSECTION	\$ -	\$ 40,300	\$ 293,400	\$ -	\$ -	\$ 333,700	
	GREYSTONE LANE/PLEASANT RIDGE RD INTERSECTION	\$ -	\$ -	\$ 265,000	\$ 661,000	\$ 1,736,000	\$ 2,662,000	
	S CENTURY DR/VENTURE LANE INTERSECTION IMPROVEMENT	\$ -	\$ -	\$ 300,000	\$ 800,000	\$ 2,000,000	\$ 3,100,000	
	NW LOWER BRIDGE WAY - NW 43RD ST TO TEATER AVE IMPROVEMENT	\$ -	\$ -	\$ -	\$ 250,000	\$ 750,000	\$ 1,000,000	
	ROSSAND RD/TRACY RD - US 97 TO DRAFTER RD	\$ 250,673	\$ -	\$ -	\$ -	\$ 300,000	\$ 1,950,000	\$ 2,500,673
	BUTLER MARKET RD - HAMEHOOK RD TO POWELL BUTTE HWY	\$ 320,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 320,000
	DICKEY RD	\$ 600,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 600,000
	OLD BEND REDMOND HWY - US 20 TO TUMALO RD	\$ 1,210,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,210,000
	HORSE BUTTE RD	\$ 460,000	\$ 410,000	\$ -	\$ -	\$ -	\$ -	\$ 870,000
	SPRING RIVER RD - S CENTURY DR TO STELLAR DR	\$ 3,000,000	\$ 410,000	\$ -	\$ -	\$ -	\$ -	\$ 3,410,000
	POWELL BUTTE HWY - MCGRATH RD TO US 20	\$ 540,000	\$ 500,000	\$ -	\$ -	\$ -	\$ -	\$ 1,040,000
	NORTHWEST WAY - NW COYNER AVE TO NW ALTMETER WAY	\$ -	\$ 2,900,000	\$ -	\$ -	\$ -	\$ -	\$ 2,900,000
	BILLADEAU RD	\$ -	\$ 815,000	\$ 556,000	\$ -	\$ -	\$ -	\$ 1,371,000
	SKYLINE RANCH RD - CENTURY DR TO CITY LIMITS	\$ -	\$ 580,000	\$ 500,000	\$ -	\$ -	\$ -	\$ 1,080,000
	DESCHUTES MKT RD/TUMALO RD TO 19TH ST TO TUMALO PL	\$ -	\$ 500,000	\$ 1,320,000	\$ -	\$ -	\$ -	\$ 1,820,000
	TUMALO RD	\$ -	\$ -	\$ 420,000	\$ -	\$ -	\$ -	\$ 420,000
	DORRANCE MEADOW RD	\$ -	\$ 1,090,000	\$ -	\$ -	\$ -	\$ -	\$ 1,090,000
	INDIAN FORD RD	\$ -	\$ -	\$ 1,420,000	\$ -	\$ 2,750,000	\$ -	\$ 4,170,000
	3RD ST/WALKER ST/PENGRAS ST/5TH ST (LA PINE)	\$ -	\$ -	\$ -	\$ -	\$ 580,000	\$ -	\$ 580,000
6TH ST (LA PINE)	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,210,000	\$ 1,210,000	
WARD RD - STEVENS RD TO GOSNEY RD	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,700,000	\$ 1,700,000	
SLURRY SEAL	\$ 300,000	\$ 300,000	\$ 300,000	\$ 300,000	\$ 300,000	\$ 300,000	\$ 1,500,000	
MISC LOCAL ROAD PAVEMENT PRESERVATION	\$ 200,000	\$ 200,000	\$ 200,000	\$ 200,000	\$ 200,000	\$ 200,000	\$ 1,000,000	
TRAFFIC SAFETY	\$ 150,000	\$ 150,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 600,000	
GUARDRAIL IMPROVEMENTS	\$ 150,000	\$ 150,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 100,000	\$ 600,000	
SMITH ROCK WAY BRIDGE #15452 REPLACEMENT	\$ 1,417,429	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 1,417,429	
GRIEBLING RD BRIDGE #17C30 REPLACEMENT (ODOT LBP/SFLP)	\$ 704,116	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 704,116	
HAMEHOOK RD BRIDGE #17C32 REPLACEMENT	\$ 595,000	\$ 1,200,000	\$ -	\$ -	\$ -	\$ -	\$ 1,795,000	
WILCOX AVE BRIDGE #2171-03 & -04 REMOVAL	\$ 160,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 160,000	
LOCAL ACCESS ROAD BRIDGES	\$ 150,000	\$ 250,000	\$ 250,000	\$ 250,000	\$ 250,000	\$ -	\$ 900,000	
S CENTURY DR BRIDGE #16181 REHABILITATION	\$ -	\$ 100,000	\$ 1,010,000	\$ 1,000,000	\$ -	\$ -	\$ 2,110,000	
BURGESS RD BRIDGE #09C783 REPLACEMENT	\$ -	\$ -	\$ 100,000	\$ 565,000	\$ 1,453,000	\$ -	\$ 2,118,000	
SPRING RIVER RD (HARPER) BRIDGE #17923 REHABILITATION	\$ -	\$ -	\$ -	\$ 100,000	\$ 430,000	\$ -	\$ 530,000	
	FY TOTAL CIP PROJECT COST	\$ 23,934,730	\$ 12,408,700	\$ 18,087,100	\$ 10,956,000	\$ 11,929,000	\$ 77,315,530	
	FY TOTAL - TRANSPORTATION SYSTEM	\$ 13,757,512	\$ 4,963,700	\$ 10,721,100	\$ 5,011,000	\$ 6,436,000	\$ 40,889,312	
	FY TOTAL - PAVEMENT PRESERVATION	\$ 6,850,673	\$ 5,595,000	\$ 5,806,000	\$ 3,830,000	\$ 3,410,000	\$ 25,491,673	
	FY TOTAL - TRAFFIC SAFETY	\$ 300,000	\$ 300,000	\$ 200,000	\$ 200,000	\$ 200,000	\$ 1,200,000	
	FY TOTAL - BRIDGE CONSTRUCTION	\$ 3,026,545	\$ 1,550,000	\$ 1,360,000	\$ 1,915,000	\$ 1,883,000	\$ 9,734,545	

APPENDIX B

FY 2024-2028 CIP PROGRAM MAPS



DESCHUTES COUNTY ROAD DEPARTMENT CAPITAL IMPROVEMENT PROJECTS 5-YEAR PLAN

Fiscal Years 2024 - 2028

TRANSPORTATION SYSTEM

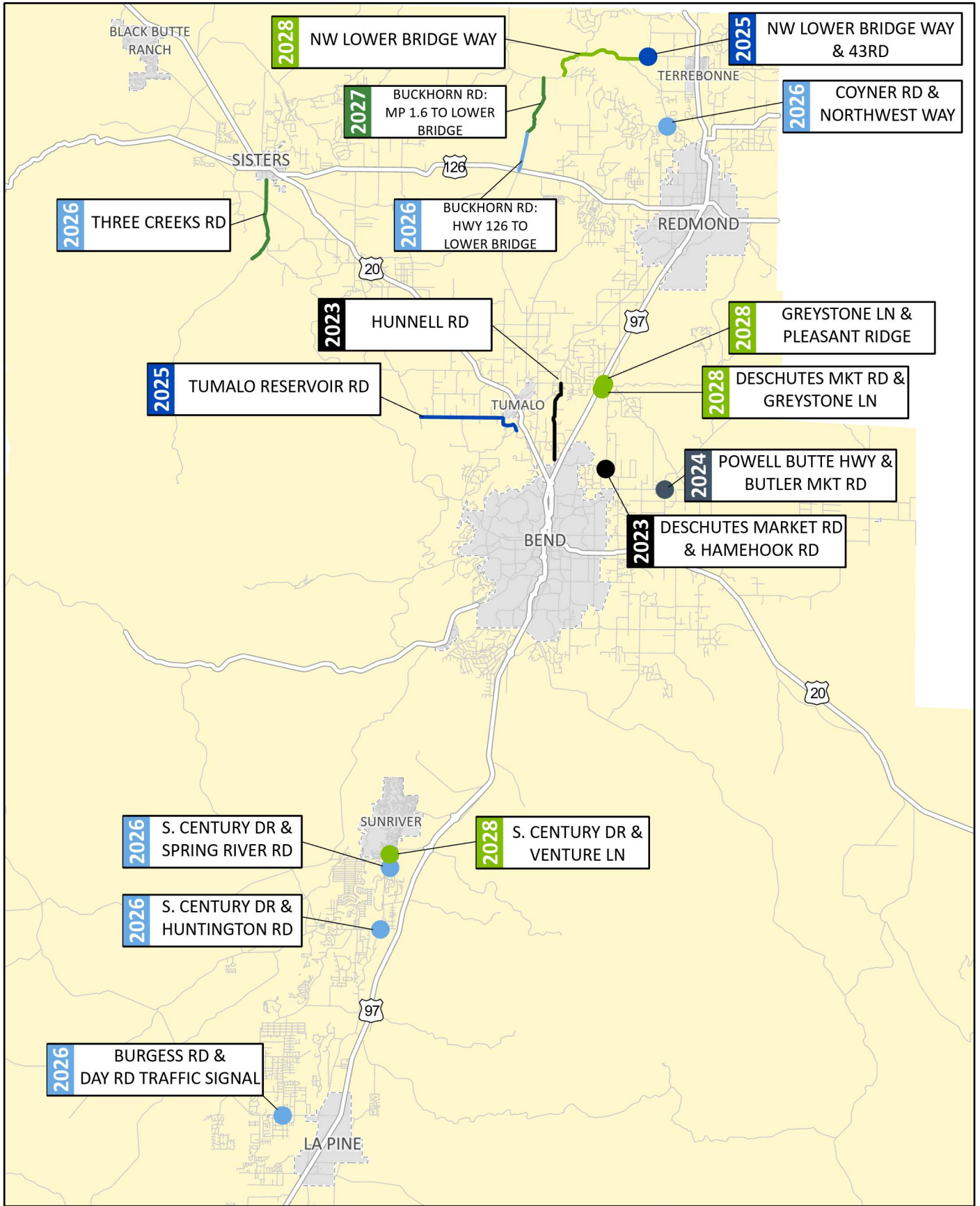
YEAR	NAME
2023:	Hunnell Rd; Loco Rd to Tumalo Rd Improvement
2023:	Deschutes Market Rd / Hamhook Rd Roundabout
2024:	Powell Butte Hwy / Butler Market Roundabout
2024:	La Pine UIC Stormwater Improvements
2025:	NW Lower Bridge Way / 43rd Intersection Improvement
2025:	Tumalo Reservoir Rd; O.B. Riley Rd to Sisemore Rd
2026:	S. Century Dr & Spring River Rd Roundabout
2026:	S. Century Dr & Huntington Rd Roundabout
2026:	Coyner Road / Northwest Way Intersection
2026:	Burgess Road / Day Road Traffic Signal
2026:	Buckhorn Rd; Hwy 126 - MP 1.6 (FLAP)
2026:	Three Creeks Rd; Sisters City Limits-FS Boundary (FLAP)
2027:	Buckhorn Rd; MP 1.6 - Lower Bridge Way
2028:	Deschutes Market Rd / Greystone Ln Intersection Improvement
2028:	Greystone Ln / Pleasant Ridge Intersection
2028:	S. Century Dr / Venture Ln Intersection Improvement
2028:	NW Lower Bridge Way; 43rd to Teater Ave Improvement

PAVEMENT PRESERVATION

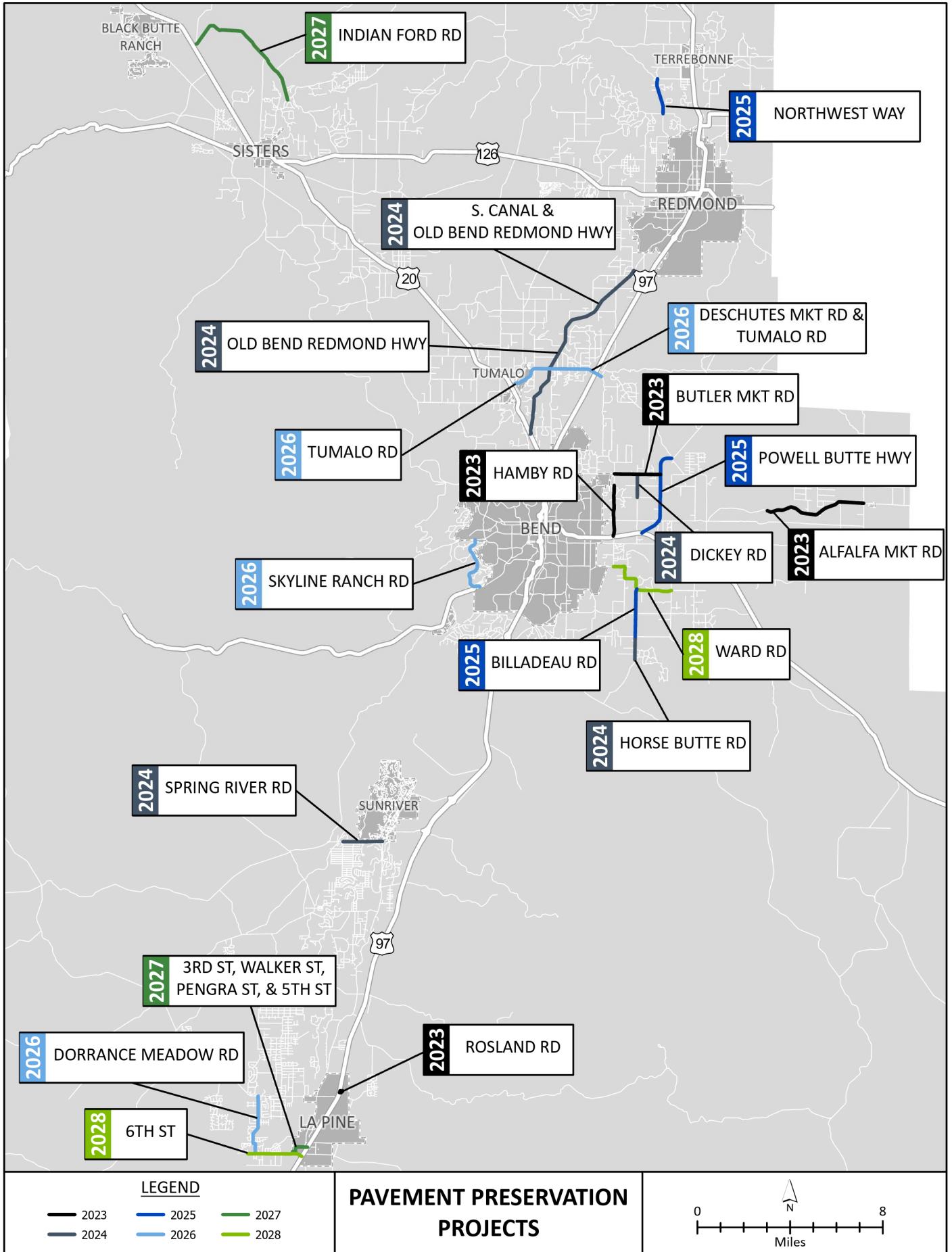
YEAR	NAME
2023:	Hamby Rd; US 20 to Butler Market Rd
2023:	Alfalfa Mkt Rd; MP 4 to Johnson Ranch Rd / Willard Rd
2023:	Rosland Rd / Tracy Rd; US 97 to Drafter Rd
2023:	Butler Market Rd; Hamhook Rd to Powell Butte Hwy
2024:	Dickey Rd
2024:	Old Bend Redmond Hwy; US20 to Tumalo Rd
2024:	Horse Butte Rd
2024:	S. Canal & Old Bend Redmond Hwy; Tumalo Rd to Helmholtz Way
2024:	Spring River Rd; S Century Dr to Stellar Dr
2025:	Powell Butte Hwy; McGrath Rd to US 20
2025:	Northwest Way; NW Coyner Ave to NW Almeter Way
2025:	Billadeau Rd
2026:	Skyline Ranch Rd; Century Dr to City Limits
2026:	Deschutes Market Rd & Tumalo Rd; 19th St to Tumalo Pl
2026:	Tumalo Rd
2026:	Dorrance Meadow Rd
2027:	Indian Ford Rd
2027:	3rd St, Walker St, Pengra St, & 5th St (La Pine)
2028:	6th St (La Pine)
2028:	Ward Rd; Stevens Rd to Gosney Rd

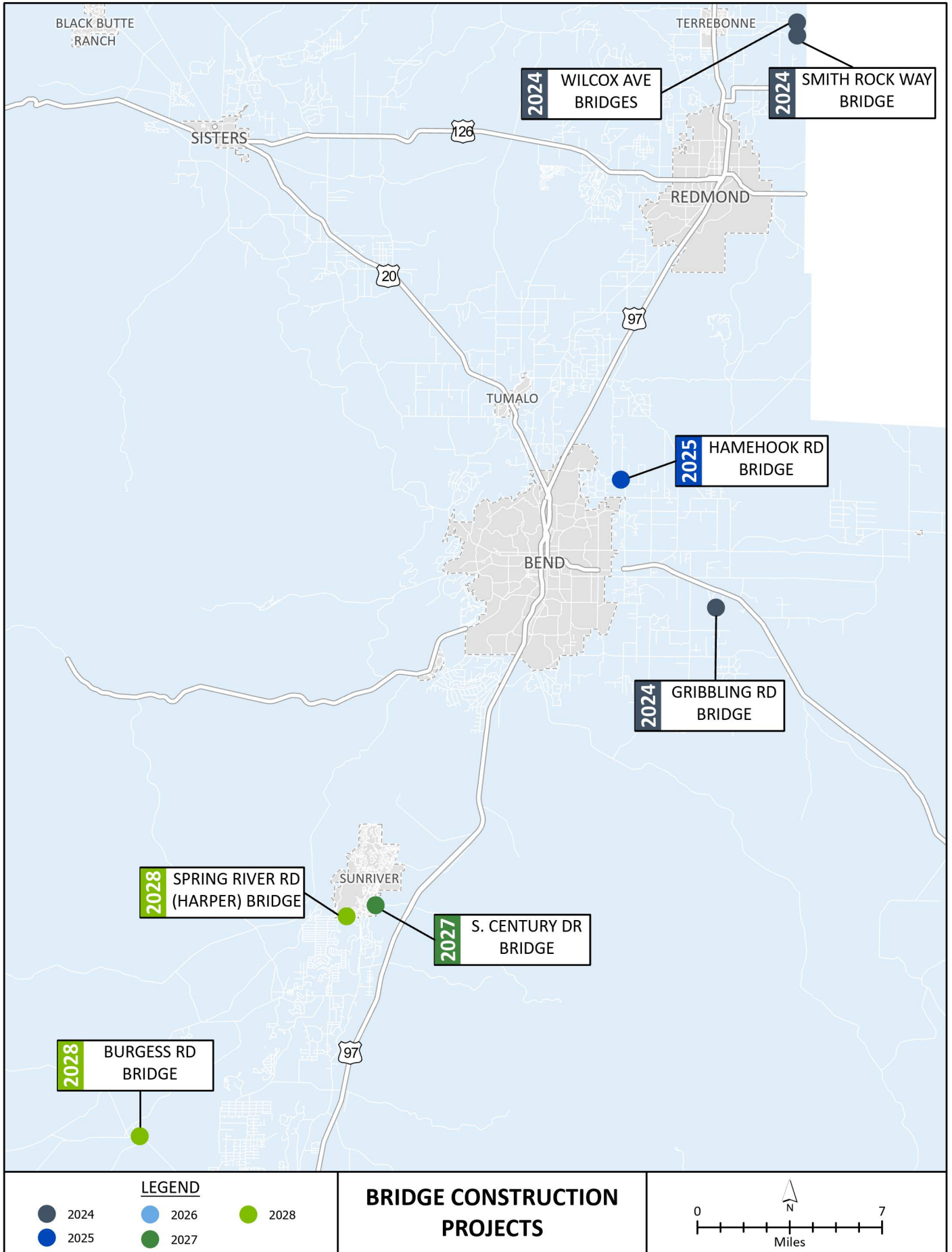
BRIDGE CONSTRUCTION

YEAR	NAME
2024:	Smith Rock Way Bridge
2024:	Gribbling Rd Bridge
2024:	Wilcox Ave Bridges
2025:	Hamhook Rd Bridge
2027:	S. Century Dr Bridge
2028:	Burgess Rd Bridge
2028:	Spring River Rd (Harper) Bridge



<p>LEGEND</p> <p> — 2023 — 2025 — 2027 — 2024 — 2026 — 2028 </p>			<p>TRANSPORTATION SYSTEM PROJECTS</p>	<p>0 8 Miles</p>
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APPENDIX C

PROJECT INFORMATION SHEETS AND LOCATION MAPS

Hunnell Road: Loco Road to Tumalo Road

Hunnell Road between the northerly Bend city limit at Loco Road and Tumalo Road is a north-south rural collector roadway as identified in the Deschutes County Transportation System Plan (TSP). From Loco Road to Rogers Road, Hunnell Road is an unimproved dirt road only passable by high-clearance vehicles, and is a combination of gravel and paved road of variable width from Rogers Road to Tumalo Road. The road serves traffic from various lower-order roads and adjacent properties and distributes that traffic to higher-order arterial roads and principal highways.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Functional Classification: Rural Collector
- ADT: 369 (2022)

SCOPE OF WORK

- Reconstructing/widening the roadway to a paved width of 30 feet with 2 to 4-foot-wide aggregate shoulders
- Realigning Hunnell Road at the intersection with Pohaku Road
- Constructing a new crossing structure at the Swalley Irrigation Canal
- Removal of roadside hazards
- Installing concrete splitter islands and dynamic speed feedback systems
- Installing new pavement markings and delineation
- Improving the approach to Tumalo Road
- Legalizing the right-of-way where necessary

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$162,815	\$49,800	\$212,615
RIGHT OF WAY	\$55,000	-	\$55,000
CONSTRUCTION	\$3,800,000	\$1,520,000	\$5,320,000
TOTAL	\$4,017,815	\$1,569,800	\$5,587,615

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								

HUNNELL ROAD:

LOCO ROAD TO TUMALO ROAD IMPROVEMENT

CONCEPT PLAN

NOVEMBER 4, 2020



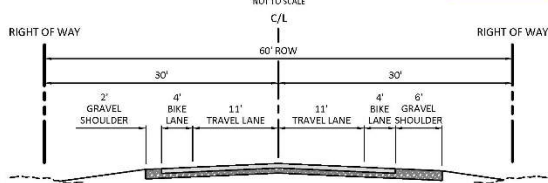
ROAD DEPARTMENT



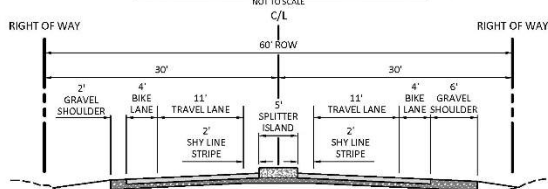
SCALE:
1" = 1,200'

1 IMPROVED ROADWAY SECTION

TYPICAL ROADWAY SECTION



RAISED MEDIAN ROADWAY SECTION



EXAMPLE SPLITTER ISLAND (SKYLINERS ROAD)

2 IMPROVED POHAKU ROAD INTERSECTION ALIGNMENT

3 NEW CANAL CROSSING STRUCTURE

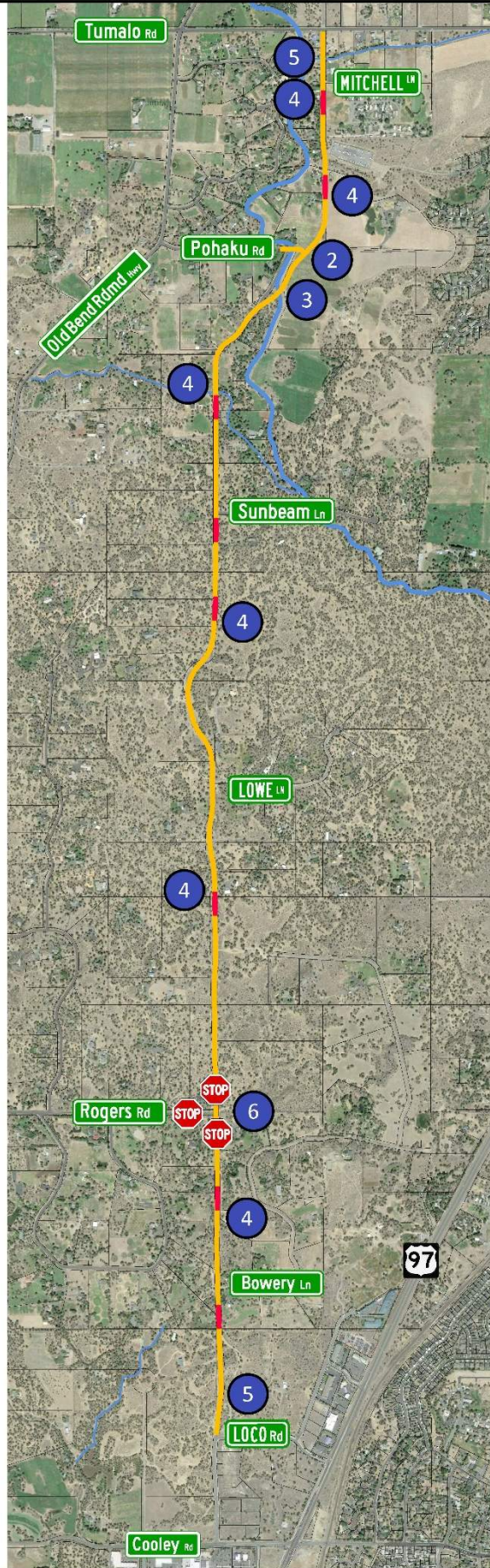
4 DYNAMIC SPEED FEEDBACK SIGNS

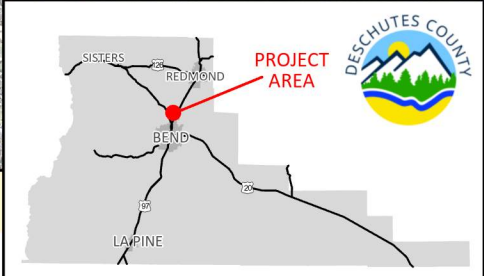
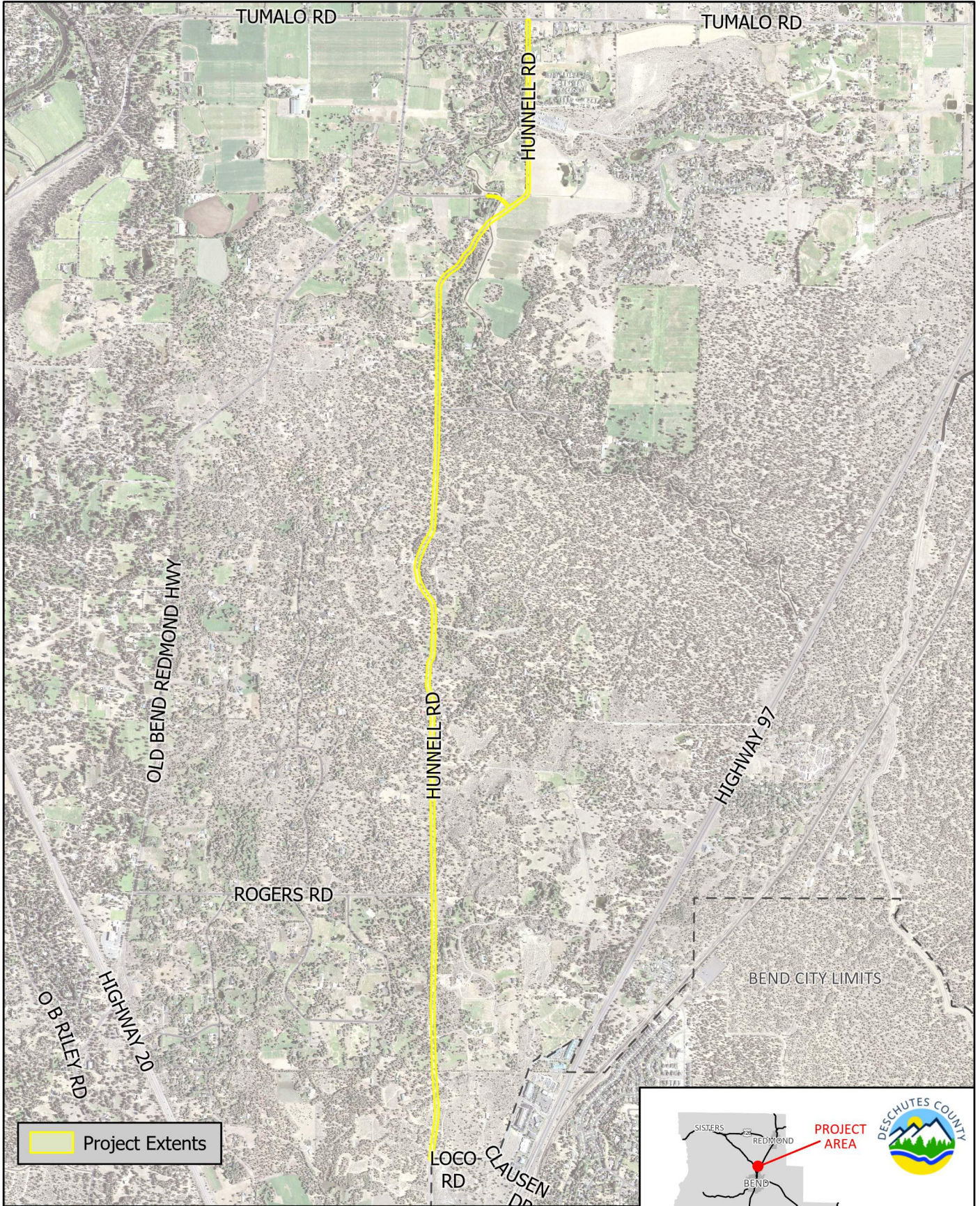


5 "NO THRU TRUCKS" SIGNAGE



6 ALL-WAY STOP AT ROGERS ROAD





2023

TRANSPORTATION SYSTEM PROJECT

HUNNELL RD: LOCO RD TO TUMALO RD

Deschutes Market Road / Hamehook Road Roundabout

Deschutes Market Road is a north-south arterial and Hamehook Road is an east-west collector. Both roadway segments are located northeast of Bend. The corridor is frequently used as a bypass to US 97 from east Bend. The Deschutes Market Road/Hamehook Road intersection is presently a 3-legged intersection with stop sign-control on the north leg only. These roads have seen moderate traffic growth due to development and completion of the Deschutes Market interchange at US 97. Growth is expected to increase as development in the area continues.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: Deschutes Market Road Hamehook Road
- Functional Classification: Rural Arterial Rural Collector
- ADT: 9,312 (2022) 3,564 (2022)

SCOPE OF WORK

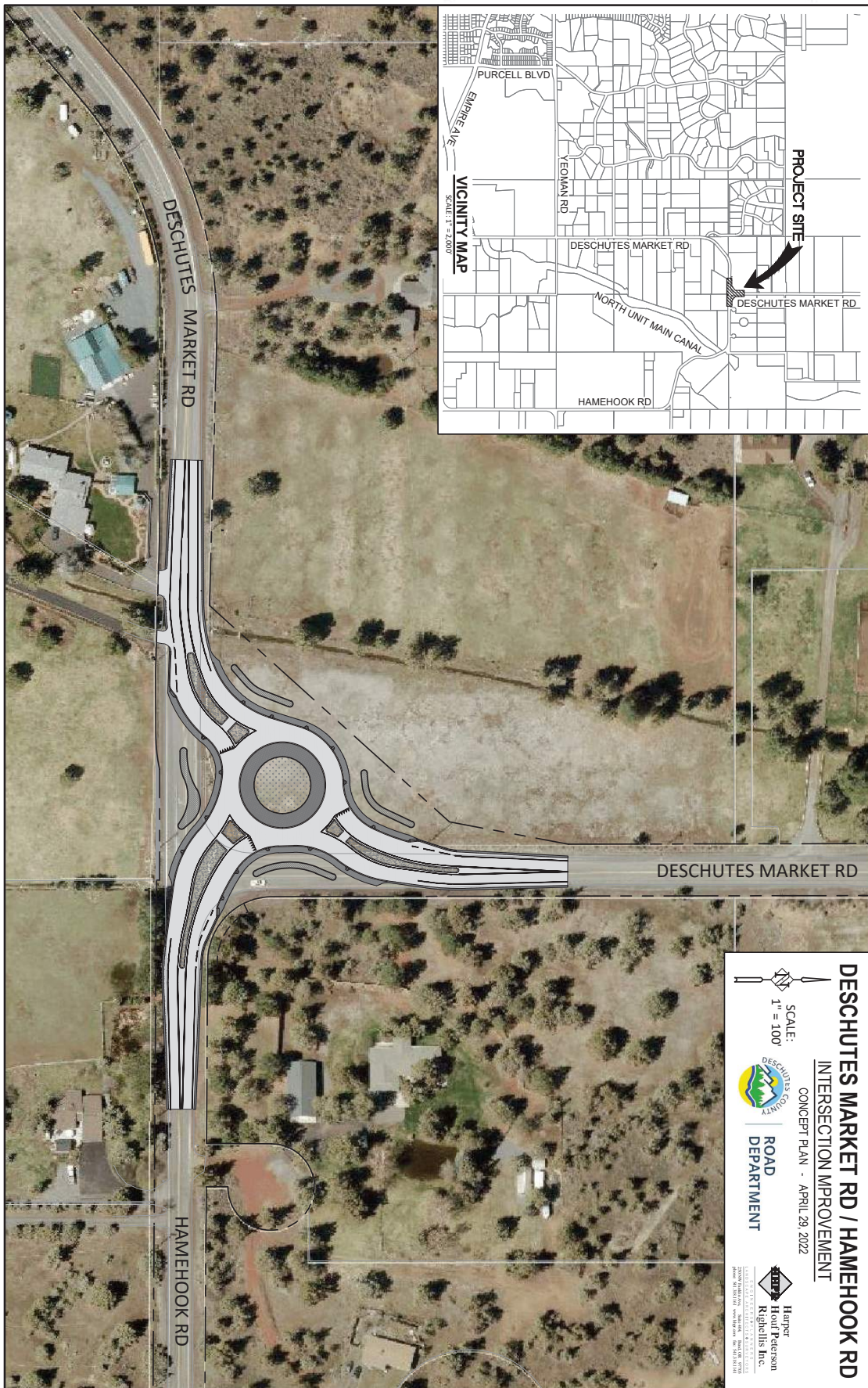
- Constructing a single-lane roundabout
- Installing illumination
- Installing new signs, striping and delineation
- Clearing roadside obstructions

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$179,780	-	\$179,780
RIGHT OF WAY	\$128,025	-	\$128,025
CONSTRUCTION	\$1,997,489	\$250,000	\$2,247,489
TOTAL	\$2,305,294	\$250,000	\$2,555,294

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



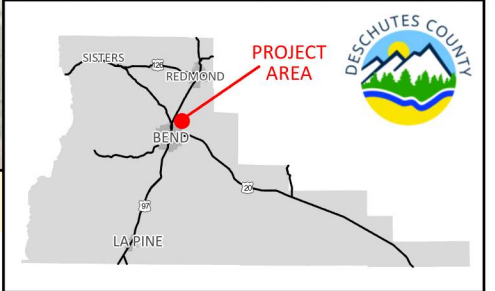


Project Extents

2023

TRANSPORTATION SYSTEM PROJECT

DESCHUTES MARKET RD / HAMEHOOK RD ROUNDABOUT



Powell Butte Highway / Butler Market Road Roundabout

Powell Butte Highway is a north-south arterial roadway connecting the area east of Bend to US 20, Crook County and the community of Powell Butte. Butler Market Rd is an east-west arterial roadway connecting the area east of Bend to Deschutes Market Rd and Hamhook Rd. The intersection is situated adjacent to the main entrance to the Bend Municipal Airport. The intersection of Powell Butte Highway and Butler Market Rd is presently a three-legged intersection with stop sign control on the eastbound approach only.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: Powell Butte Hwy Butler Market Road
- Functional Classification: Rural Arterial Rural Arterial
- ADT: 7,418 (2022) 4,008 (2022)

SCOPE OF WORK

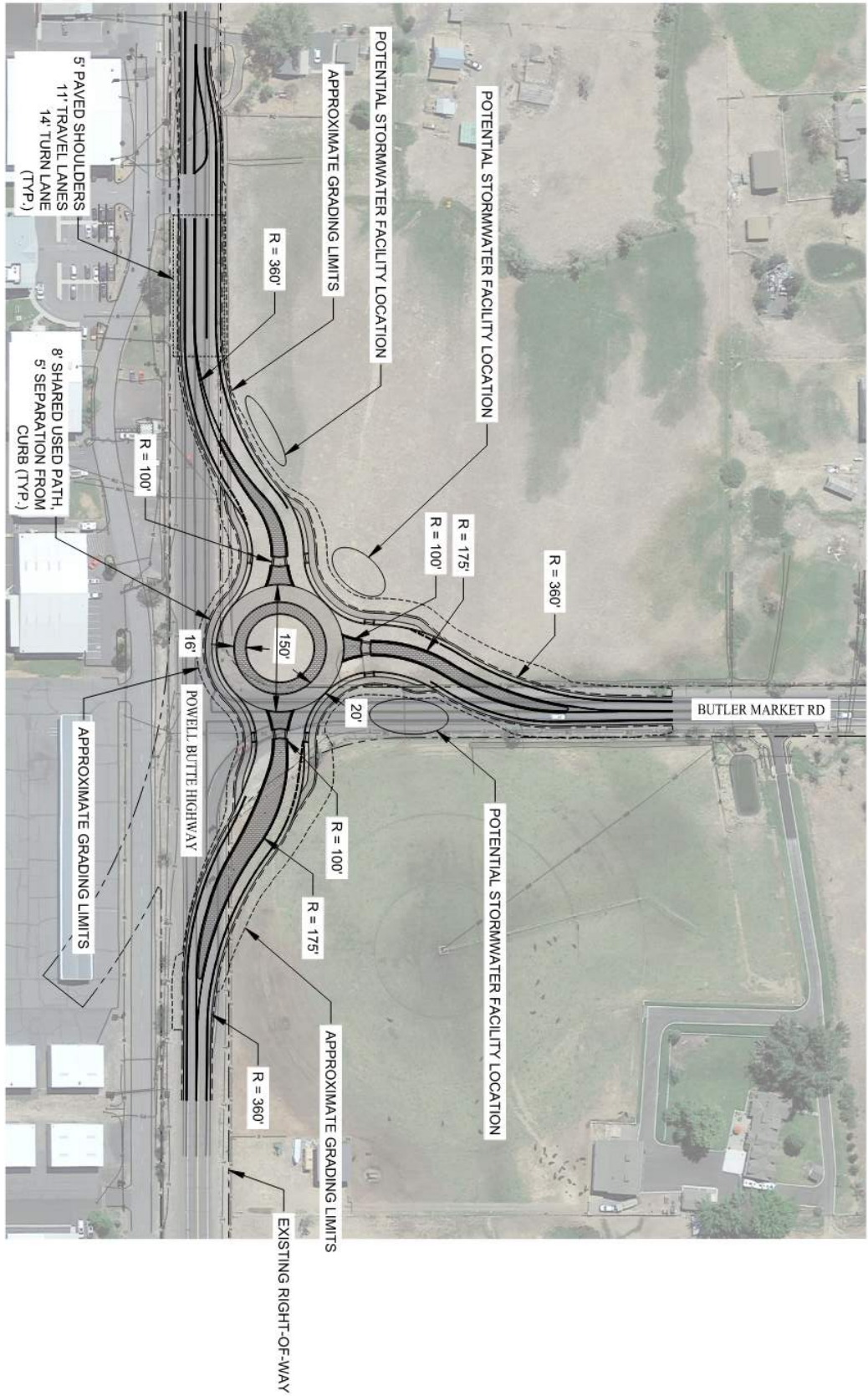
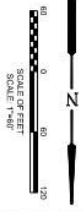
- Constructing a single-lane roundabout
- Installing illumination
- Installing new signs, striping and delineation
- Installing a protected left turn lane for the Bend Municipal Airport

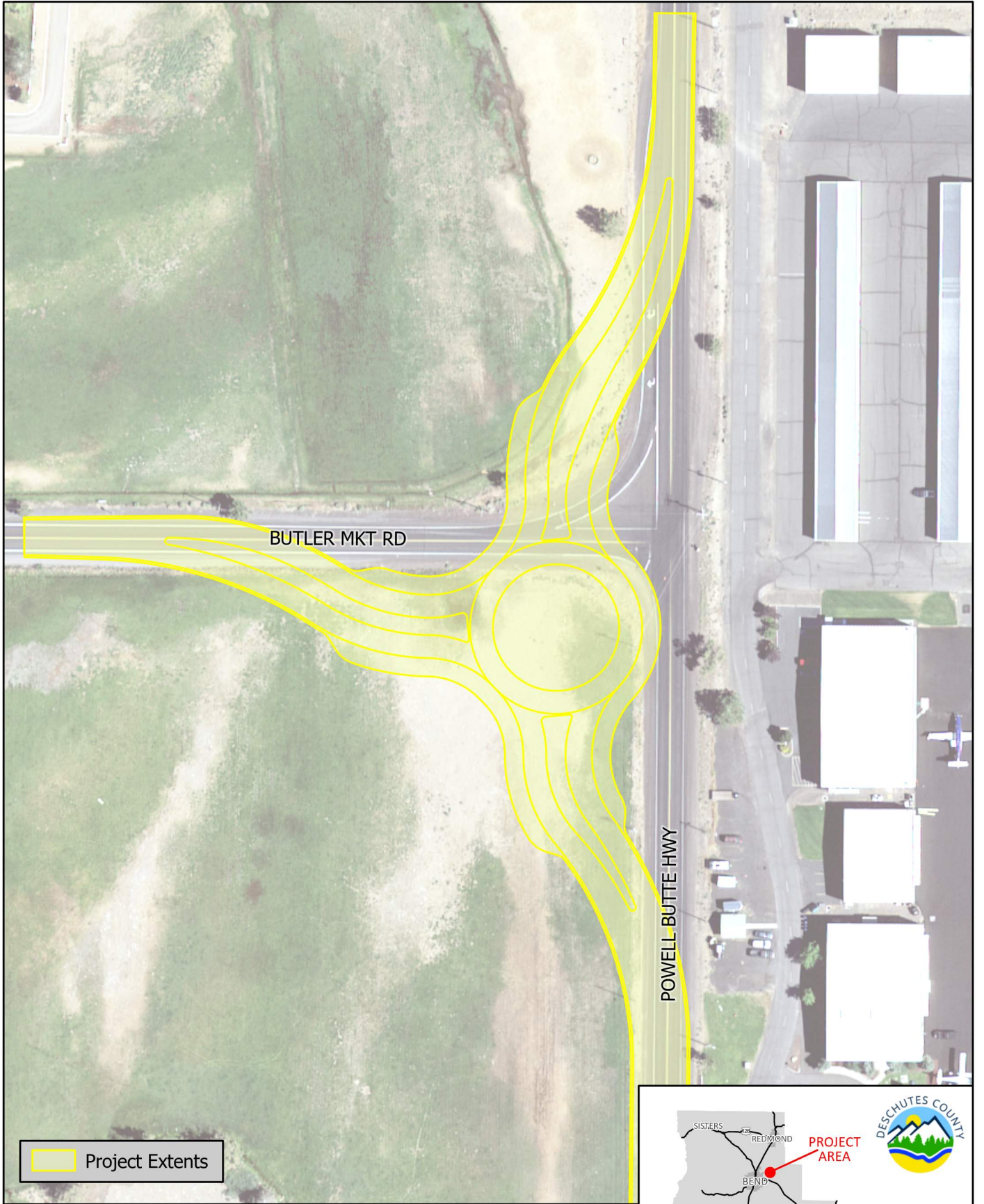
FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$306,153	\$100,000	\$406,153
RIGHT OF WAY	-	\$85,000	\$85,000
CONSTRUCTION	-	\$2,457,402	\$2,457,402
TOTAL	\$306,153	\$2,642,402	\$2,948,555

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								

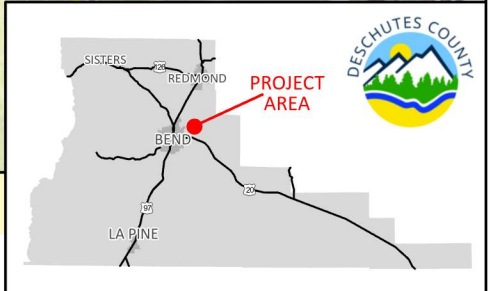




Project Extents

2024

TRANSPORTATION SYSTEM PROJECT
POWELL BUTTE HWY / BUTLER MARKET ROUNDABOUT



La Pine UIC Stormwater

Improvements

The City of La Pine is an unincorporated community south of Bend. Deschutes County Road Department has identified several Underground Injection Control (UIC) Stormwater facilities that require upgrades. These improvements are necessary to maintain appropriate catchment volume and water quality for storm runoff within the City of La Pine.



- Project Justification: Department of Environmental Qualifications (DEQ) Underground Injection Control (UIC) Permit Requirements
- Road Name: Various
- Functional Classification: Various

SCOPE OF WORK

- Constructing new Underground Injection Control (UIC) facilities and stormwater conveyance structures
- Installing new stormwater inlets

FUNDING

	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$60,000	\$60,000
RIGHT OF WAY	-	-
CONSTRUCTION	\$180,000	\$180,000
TOTAL	\$240,000	\$240,000

SCHEDULE

	<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				

NW Lower Bridge Way / 43rd Street Intersection Improvement

NW Lower Bridge Way is an east-west rural collector connecting the area west of Terrebonne to US 97. NW 43rd Street is a major collector and serves as the primary access route for more than 5,000 residents of Crooked River Ranch. NW 43rd Street intersects NW Lower Bridge Way at a three-legged intersection with stop sign control on the north leg only, where there has been a history of crashes. The intersection is a primary node for both recreational and residential traffic, and is bisected by the Sisters to Smith Rock Scenic Bikeway.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: NW Lower Bridge Way NW 43rd Street
- Functional Classification: Rural Collector Rural Collector
- ADT: 7,723 (2022) 6,781 (2022)

SCOPE OF WORK

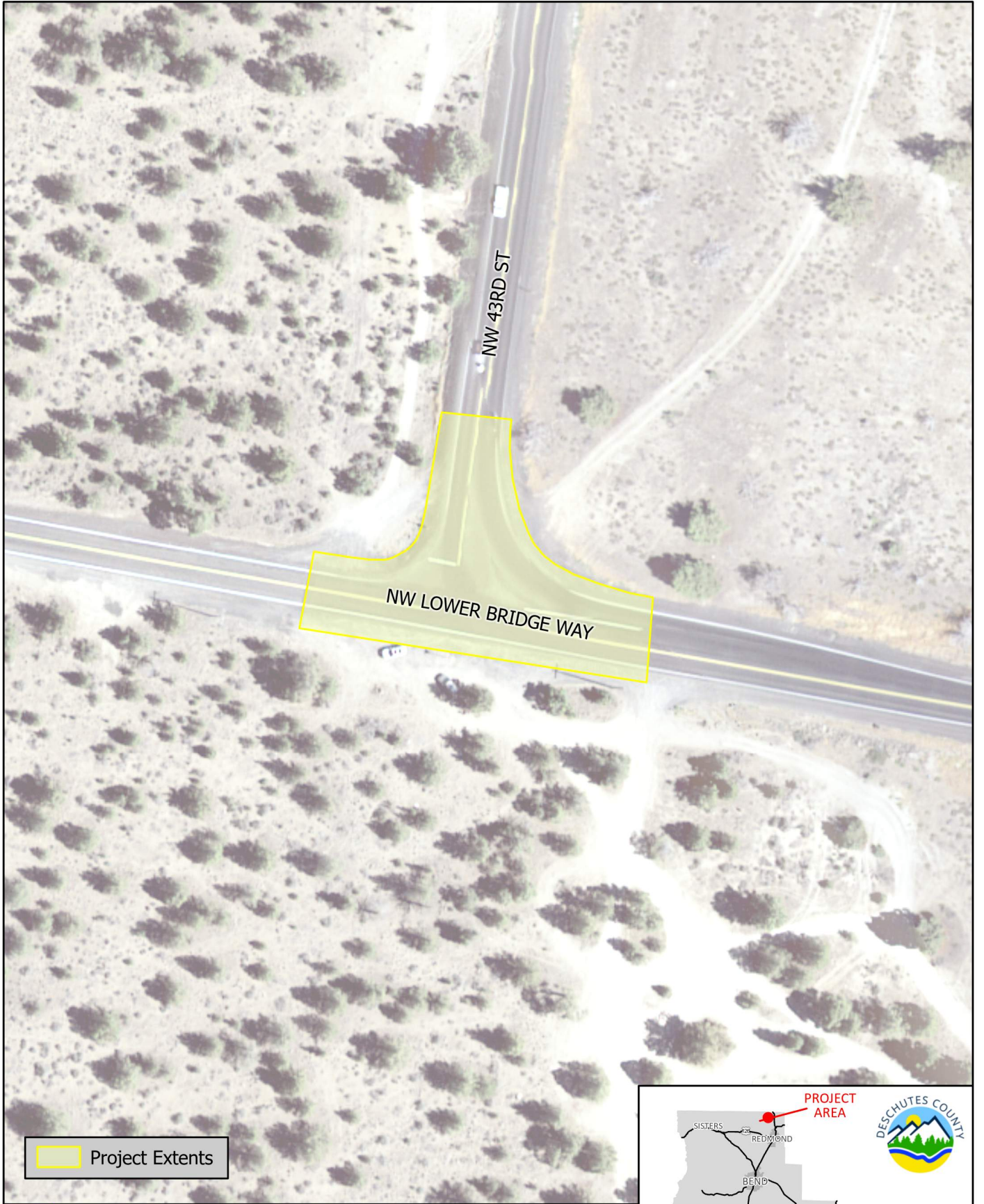
- Conducting a safety and capacity analysis at the intersection of NW Lower Bridge Way / NW 43rd Street
- Revising intersection geometry to improve safety for vehicular and bicycle traffic
- Installing new pavement markings and signage

FUNDING

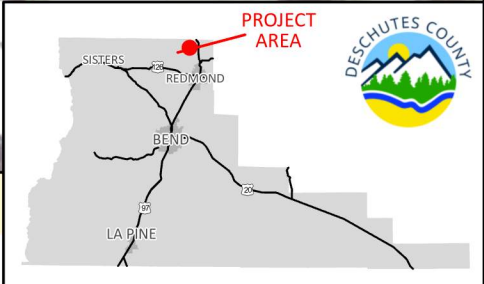
	<i>FY 2023</i>	<i>FY 2024</i>	<i>FY 2025</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$60,000	\$290,000	-	\$350,000
RIGHT OF WAY	-	-	-	-
CONSTRUCTION	-	\$1,000,000	\$1,400,000	\$2,400,000
TOTAL	\$60,000	\$1,290,000	\$1,400,000	\$2,750,000

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>				<i>FY 2025</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



Project Extents



2025 **TRANSPORTATION SYSTEM PROJECT**
NW LOWER BRIDGE WAY/ 43RD ST INTERSECTION IMPROVEMENT

Tumalo Reservoir Road:
O.B. Riley Road to Sisemore Road

Tumalo Reservoir Road is located southwest of Tumalo and spans from O.B. Riley Road to Sisemore Road near Tumalo Reservoir. The existing pavement width ranges from 22 to 25 feet and has a Pavement Condition Index (PCI) ranging from 74 to 79 out of 100. The existing pavement has poor ride quality and is exhibiting significant shoulder deterioration. The segment between O.B. Riley Road and Bailey Road serves as part of the Twin Bridges Scenic Bikeway.



- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
Pavement Condition Index (PCI) Rating – 75 out of 100
- Functional Classification: Rural Collector
- ADT: 1,234 (2022)

SCOPE OF WORK

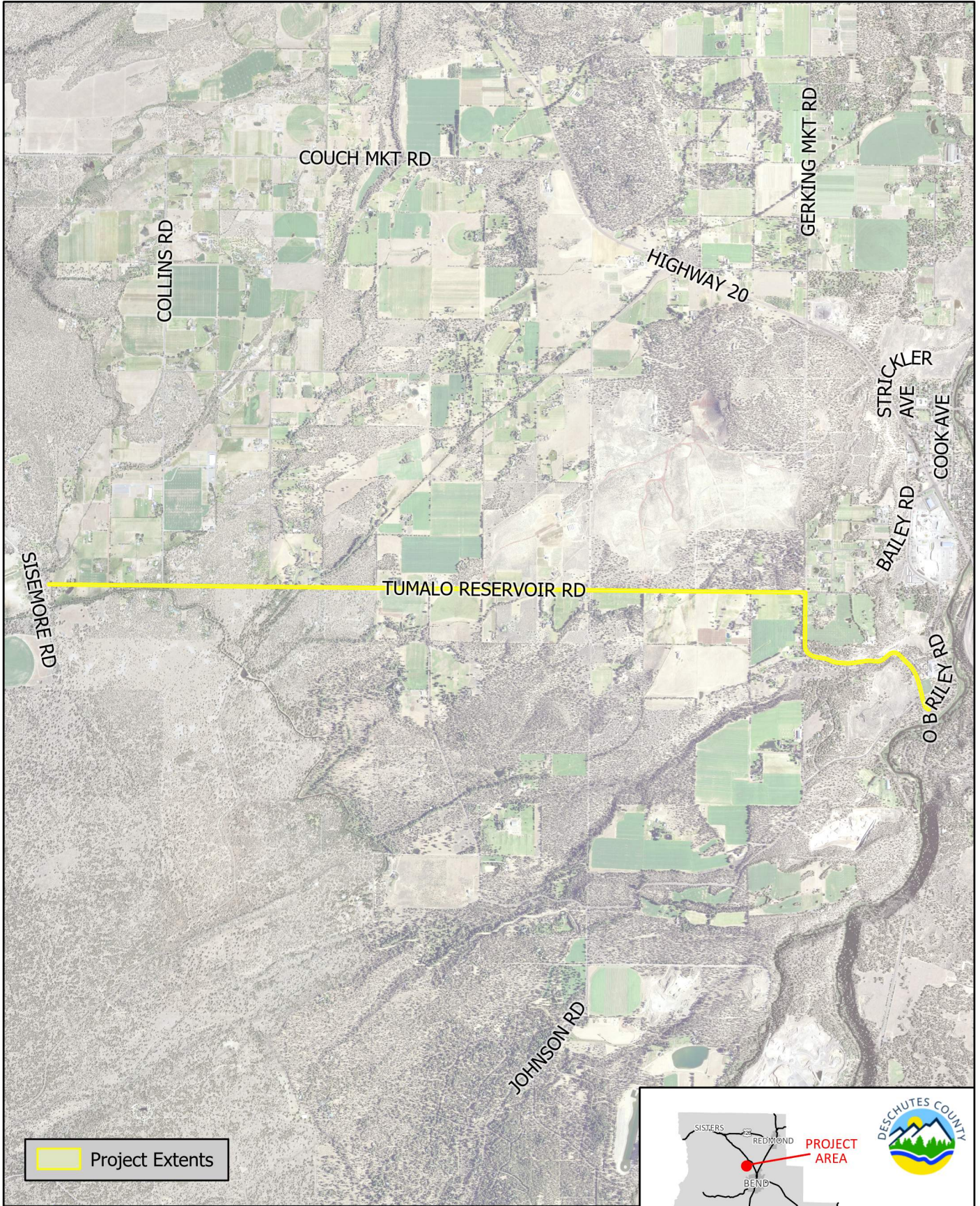
- Widening the roadway to a paved width of 30 feet with 2-foot-wide aggregate shoulders to accommodate paved bikeways
- Paving of Tumalo Reservoir Road between O.B. Riley Road and Sisemore Road
- Safety improvements – Signing, Striping and Delineation
- Removal of roadside obstructions

FUNDING

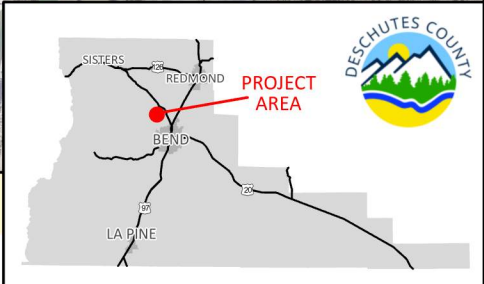
	<i>FY 2023</i>	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$35,000	\$300,000	\$378,000	-	\$713,000
RIGHT OF WAY	-	-	\$156,000	-	\$156,000
CONSTRUCTION	-	-	\$1,000,000	\$3,489,000	\$4,489,000
TOTAL	\$35,000	\$300,000	\$1,534,000	\$3,489,000	\$5,358,000

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>				<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING																
RIGHT OF WAY																
CONSTRUCTION																



 Project Extents



2025

TRANSPORTATION SYSTEM PROJECT
 TUMALO RESERVOIR RD: O.B. RILEY RD TO SISEMORE RD

South Century Drive / Spring River Road Roundabout

South Century Drive is a rural arterial that provides connectivity from communities south of Sunriver to US97, and provides recreational access to Forest Service Road 42. Spring River Road is an east-west rural arterial that connects Sunriver to River Summit Drive 4 miles to the east. The South Century Drive and Spring River Road intersection is a 3-legged intersection and presently is stop sign-controlled on the south leg only. The intersection has a history of non-fatal crashes.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: South Century Drive Spring River Road
- Functional Classification: Rural Arterial Rural Arterial
- ADT: 10,401 (2022) 5,599 (2022)

SCOPE OF WORK

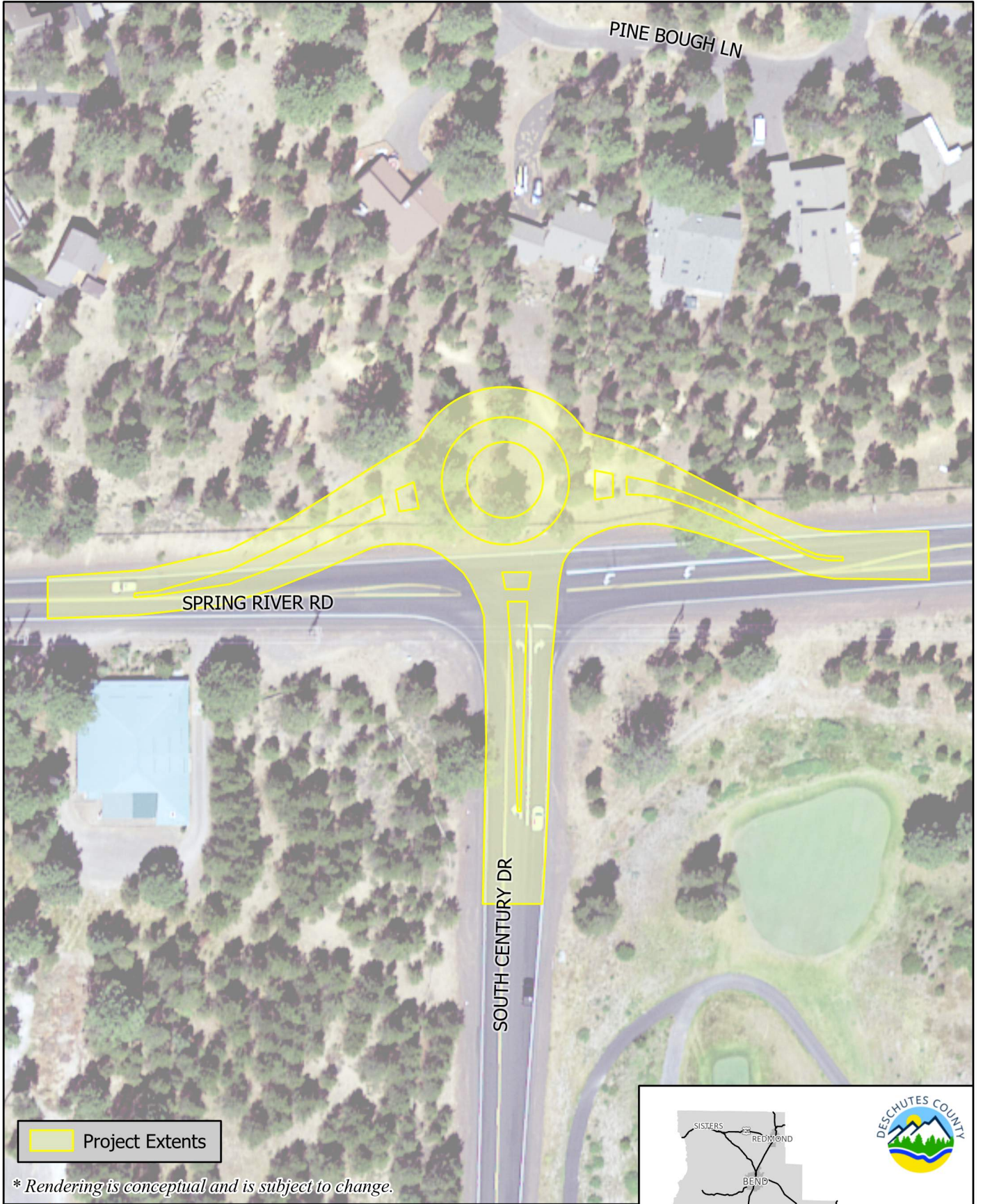
- Constructing a single-lane roundabout
- Installing illumination
- Installing new Signs, Striping and Delineation
- Clearing roadside obstructions

FUNDING

	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$177,000	\$100,000	-	\$277,000
RIGHT OF WAY	-	\$200,000	-	\$200,000
CONSTRUCTION	-	\$500,000	\$1,877,000	\$2,377,000
TOTAL	\$177,000	\$800,000	\$1,877,000	\$2,854,000

SCHEDULE

	<i>FY 2024</i>				<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



PINE BOUGH LN

SPRING RIVER RD

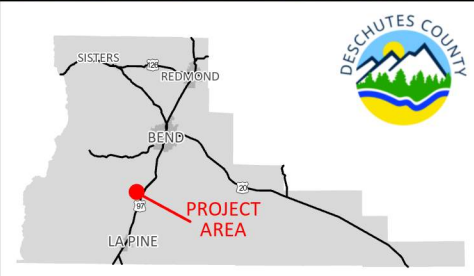
SOUTH CENTURY DR

 Project Extents


** Rendering is conceptual and is subject to change.*

2026

TRANSPORTATION SYSTEM PROJECT
S. CENTURY DR / SPRING RIVER RD ROUNDABOUT





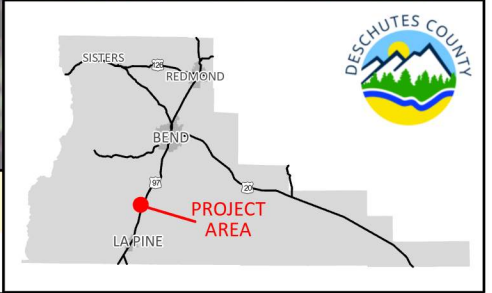
 Project Extents

** Rendering is conceptual and is subject to change.*

2026

TRANSPORTATION SYSTEM PROJECT

S. CENTURY DR / HUNTINGTON RD ROUNDABOUT



Coyner Road / Northwest Way Intersection Improvement

Northwest Way and Coyner Avenue are rural collector roadways that connect communities northwest of Redmond to the City of Redmond and US 97. The intersection is stop sign-controlled on the east and west approaches. There were nine reported crashes at the intersection during the most recent five year period of available crash data (2016-2020), including seven injury crashes.



- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: Northwest Way Coyner Avenue
- Functional Classification: Rural Collector Rural Collector
- ADT: 2,755 (2022) 2,327 (2022)

SCOPE OF WORK

- Constructing protected left-turn lanes on Northwest Way
- Performing asphalt pavement rehabilitation
- Installing new Signs, Striping and Delineation
- Clearing roadside obstructions


FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$56,000	-	\$56,000
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$367,000	\$367,000
TOTAL	\$56,000	\$367,000	\$423,000

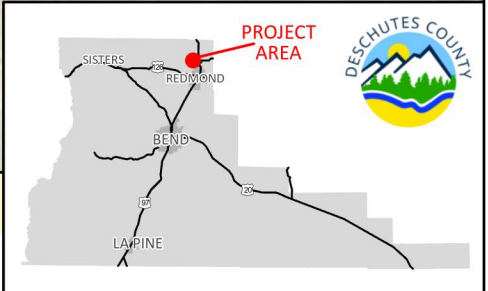
SCHEDULE

	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



 Project Extents

** Rendering is conceptual and is subject to change.*



2026

TRANSPORTATION SYSTEM PROJECT

COYNER RD / NORTHWEST WAY INTERSECTION IMPROVEMENT

Burgess Road / Day Road Traffic Signal



Burgess Road is a rural arterial that connects La Pine and US97 to Forest Service Road 42 and the Deschutes National Forest. Day Road is a north-south rural collector that connects residents north of La Pine to US 97, the City of La Pine and La Pine State Park. The intersection is currently stop sign-controlled on the north and south approaches. Turn lanes were added to the intersection on the north and south approaches in 2018. The intersection has a history of crashes (more than 20) related to the stop-controlled approaches and poor sight distance. The intersection currently lacks adequate traffic control to manage the moderately high volumes experienced by the intersection.

- Project Justification: Deschutes County TSP 2010 – 2030 (High Priority)
- Road Name: Burgess Road Day Road
- Functional Classification: Rural Arterial Rural Collector
- ADT: 6,456 (2022) 4,415 (2022)

SCOPE OF WORK

- Installing traffic signals on all approaches
- Installing illumination
- Installing new Signs, Striping and Delineation
- Clearing roadside obstructions

FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$50,000	\$52,000	\$102,000
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$694,000	\$694,000
TOTAL	\$50,000	\$746,000	\$796,000

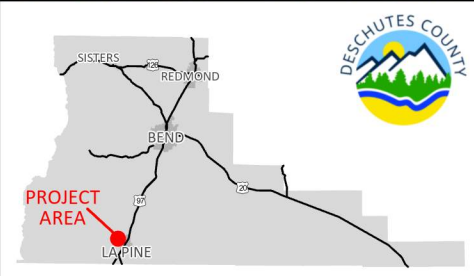
SCHEDULE

	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



 Project Extents

** Rendering is conceptual and is subject to change.*



2026

TRANSPORTATION SYSTEM PROJECT

BURGESS RD / DAY RD TRAFFIC SIGNAL

Buckhorn Road: Hwy 126 to M.P. 1.6

Buckhorn Road is an aggregate-surfaced, north-south collector roadway located west of Redmond that connects NW Lower Bridge Road to OR Hwy 126. Buckhorn Road provides access to over 100,000 acres of Federal lands and recreational areas, including the Crooked River National Grasslands and the Cline Buttes OHV area. Buckhorn Road is also a critical link for emergency services to access communities west of Terrebonne in the event of a closure on NW Lower Bridge Way. Buckhorn Road currently lacks functional characteristics and safety features of a County Collector roadway.



- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
Approved for funding under Federal Lands Access Program
- Functional Classification: Rural Collector
- ADT: 303 (2021)

SCOPE OF WORK

- Modernizing to Collector Roadway standards - Constructing 28-foot-wide asphalt concrete pavement with shoulder bikeways and 2-foot-wide aggregate shoulders from OR 126 to M.P. 1.6
- Installing new Signage, Delineation, Fencing and Marked Trail Crossings
- Removing roadside hazards and obstructions

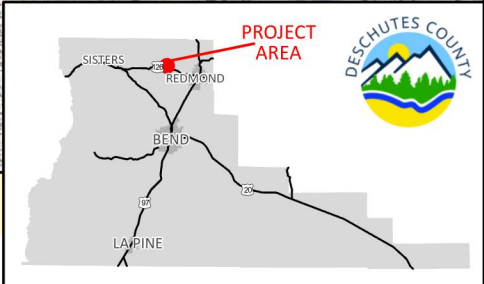
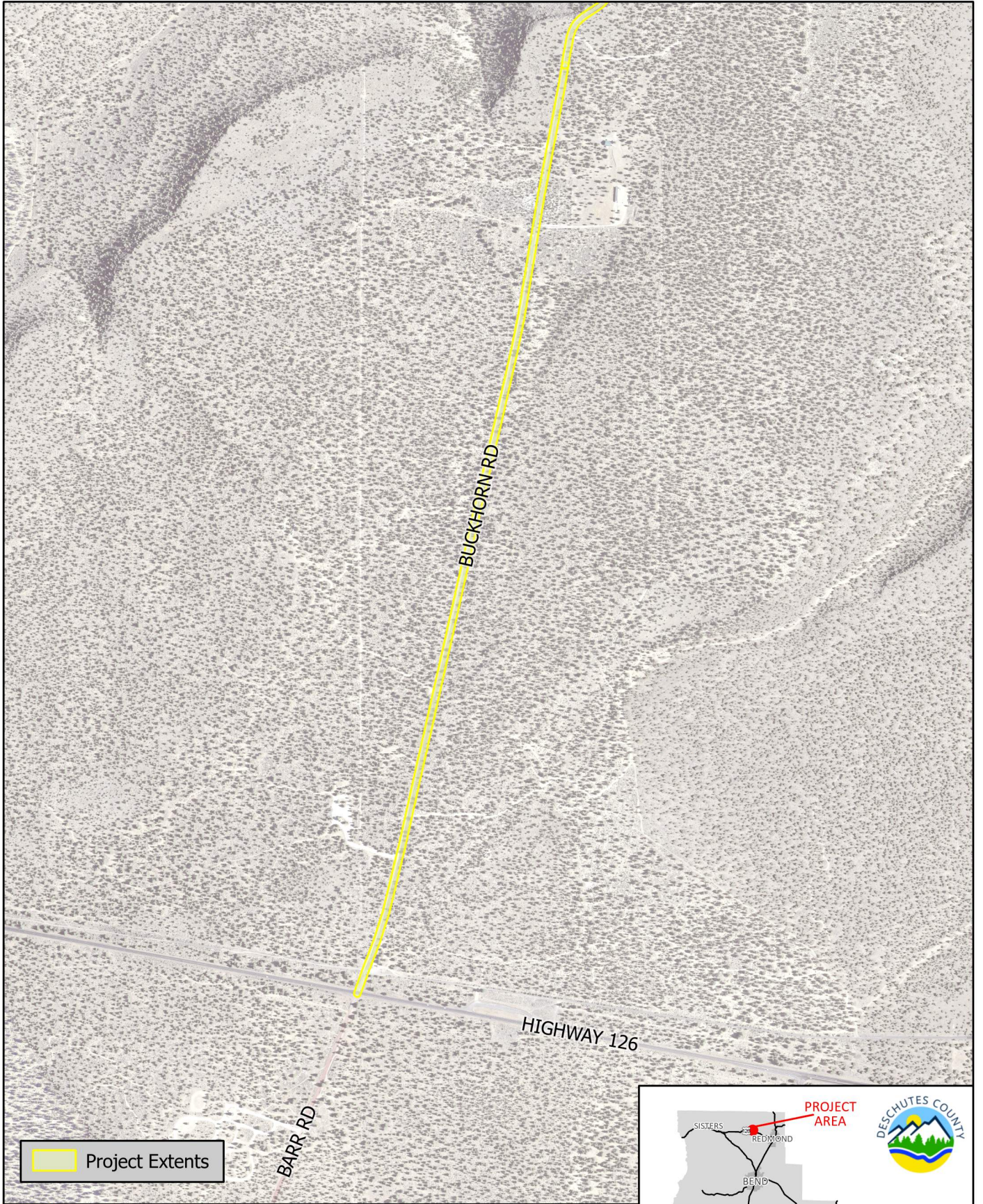
FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.*	DESCHUTES CO.*	DESCHUTES CO.*
PRELIM. ENGINEERING	\$83,400	-	\$83,400
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$565,700	\$565,700
TOTAL	\$83,400	\$565,700	\$649,100

*Project is approved for up to \$1,511,710 in FHWA Federal Lands Access Program funding. Contributions by Deschutes County are shown.

SCHEDULE

	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2026

TRANSPORTATION SYSTEM PROJECT

BUCKHORN RD: HWY 126 TO LOWER BRIDGE WAY

Three Creeks Road: Sisters City Limits to Forest Service Boundary

Three Creeks Road spans 15.85 miles north-south from the southern city limits of Sisters to Three Creeks Lake. The first 3.9 miles of Three Creeks Road, from the city limits to Forest Service Road #1600-370, is a Deschutes County rural collector road. It then becomes a Forest Service road to its terminus. Three Creeks Road primarily provides access to the Deschutes National Forest and Three Sisters Wilderness for recreational users. Three Creeks Road is a popular cyclist corridor and lacks the functional characteristics of a County bicycle route.



- Project Justification: Pavement Condition Index (PCI) Rating – 79 out of 100
Approved for funding under Federal Lands Access Program
- Functional Classification: Rural Collector
- ADT: 835 (2022)

SCOPE OF WORK

- Widening of existing roadway to a paved width of 28 feet to accommodate shoulder bikeways with 2-foot aggregate shoulders
- Performing asphalt concrete pavement preservation via overlay
- Performing asphalt pavement repair
- Installation of new Signs, Striping and Delineation
- Removal of roadside hazards and obstructions

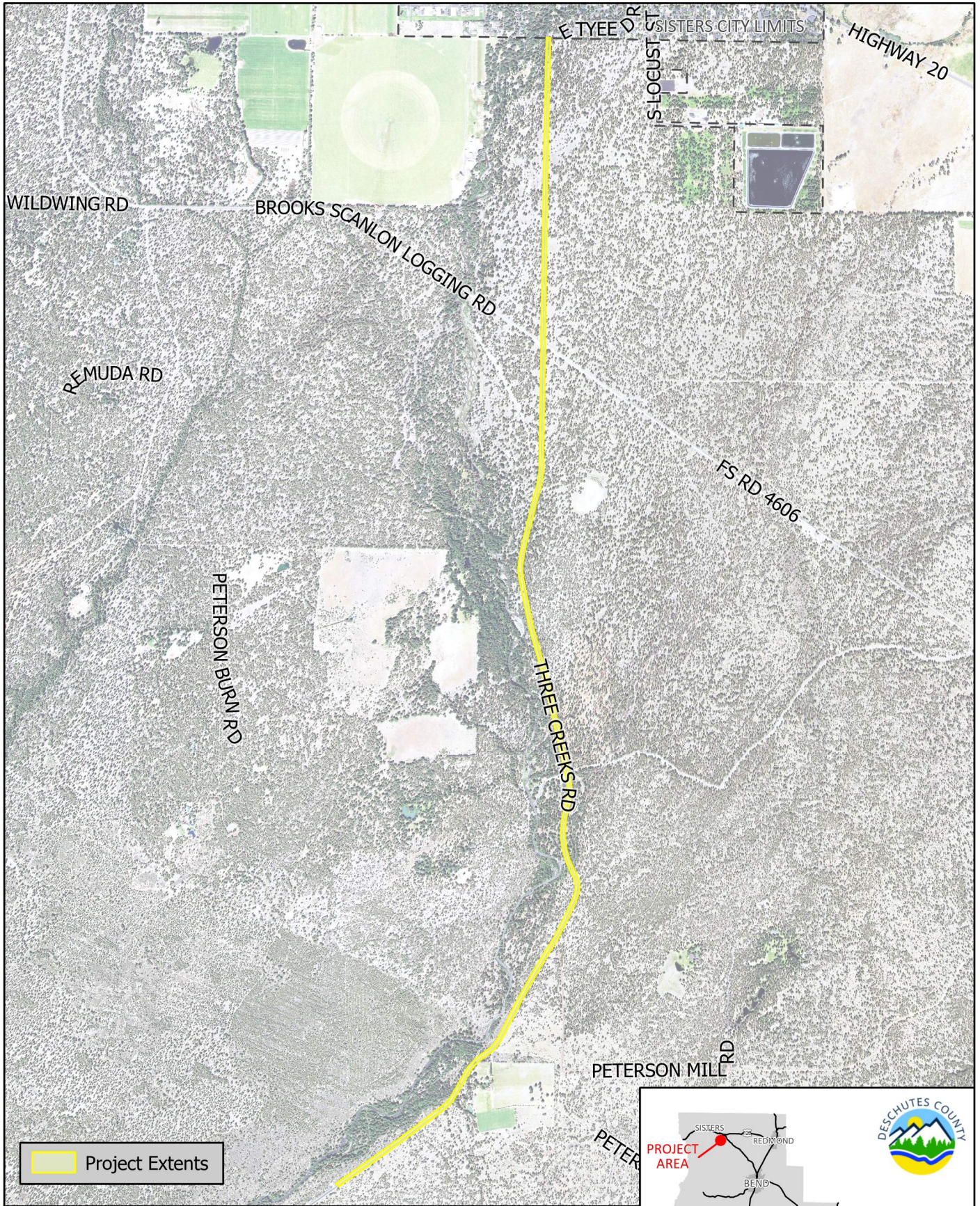
FUNDING

	<i>FY 2026</i>	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.*	DESCHUTES CO.*	DESCHUTES CO.*
PRELIM. ENGINEERING	\$40,300	-	\$40,300
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$293,400	\$293,400
TOTAL	\$40,300	\$293,400	\$333,700

*Project is approved for up to \$3,081,869 in FHWA Federal Lands Access Program funding. Contributions by Deschutes County are shown.

SCHEDULE

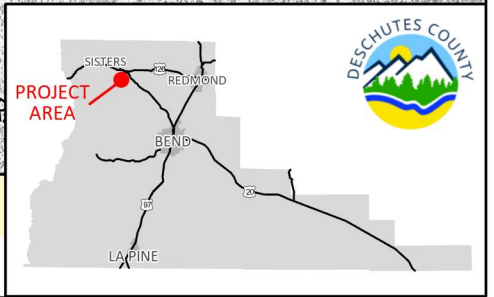
	<i>FY 2026</i>				<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2026

TRANSPORTATION SYSTEM PROJECT

THREE CREEKS RD: SISTERS CITY LIMITS TO FS BOUNDARY (FLAP)



Buckhorn Road: M.P. 1.6 to Lower Bridge Way



Buckhorn Road is an aggregate-surfaced, north-south collector roadway located west of Redmond that connects NW Lower Bridge Road to OR Hwy 126. Buckhorn Road provides access to over 100,000 acres of federal lands and recreational use areas, including the Crooked River National Grasslands and the Cline Buttes OHV area. Buckhorn Road is also a critical link for emergency services to access communities west of Terrebonne in the event of a closure on NW Lower Bridge Way. Buckhorn Road currently lacks functional and safety characteristics of a County Collector roadway.

- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
- Functional Classification: Rural Collector
- ADT: 303 (2021)

SCOPE OF WORK

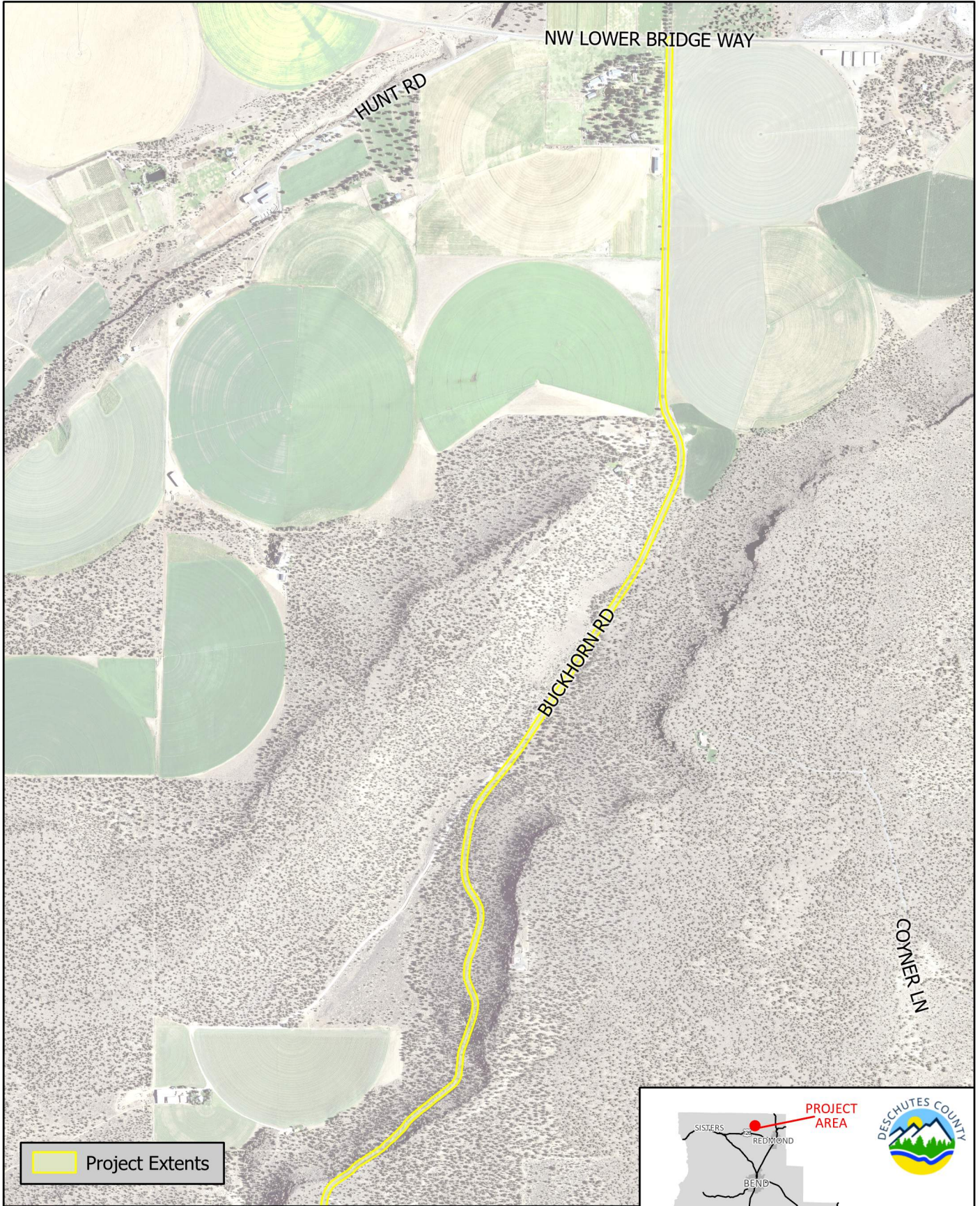
- Modernizing to Collector Roadway standards - Constructing 28-foot-wide asphalt concrete pavement with shoulder bikeways and 2-foot-wide aggregate shoulders from M.P. 1.6 to Lower Bridge Way
- Installing new Signage, Delineation, Fencing and Marked Trail Crossings
- Installing new guardrail
- Removing roadside hazards and obstructions

FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$250,000	\$100,000	-	\$350,000
RIGHT OF WAY	-	\$40,000	-	\$40,000
CONSTRUCTION	-	\$840,000	\$3,000,000	\$3,840,000
TOTAL	\$250,000	\$980,000	\$3,000,000	\$4,230,000

SCHEDULE

	<i>FY 2025</i>				<i>FY 2026</i>				<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												

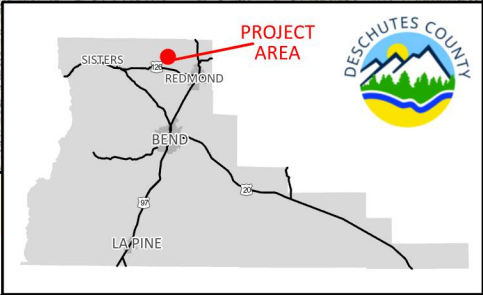


 Project Extents

2027

TRANSPORTATION SYSTEM PROJECT

BUCKHORN RD: MP 1.6 TO LOWER BRIDGE WAY



Deschutes Market Rd / Graystone Lane Intersection Improvement

Deschutes Market Road is an arterial roadway connecting communities east of Bend to US 97 and Tumalo. Graystone Lane is a collector roadway which provides access to northbound US 97 and rural properties east of US 97. The Deschutes Market Road/Graystone Lane intersection is a critical node for passenger vehicle and freight traffic, and currently lacks functional characteristics and safety features that are necessary to accommodate the high traffic volumes exhibited at the intersection. The intersection also has a history of non-fatal crashes.



- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
- Road Name: Deschutes Market Road Graystone Lane
- Functional Classification: Rural Arterial Rural Collector
- ADT: 9,571 (2022) Not Available

SCOPE OF WORK

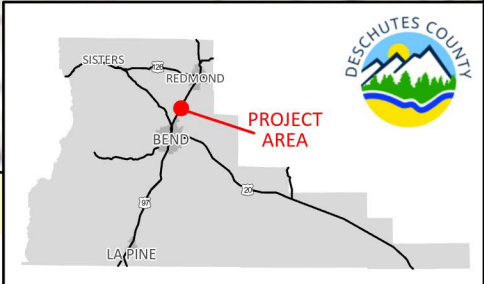
- Installing a new traffic signal
- Constructing protected turn lanes to accommodate predominant traffic movements
- Constructing new asphalt concrete pavement
- Installing new Signage, Delineation and Pavement Markings
- Installing new guardrail

FUNDING

	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$265,000	\$100,000	-	\$365,000
RIGHT OF WAY	-	\$61,000	-	\$61,000
CONSTRUCTION	-	\$500,000	\$1,736,000	\$2,236,000
TOTAL	\$265,000	\$661,000	\$1,736,000	\$2,662,000

SCHEDULE

	<i>FY 2026</i>				<i>FY 2027</i>				<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2028 **TRANSPORTATION SYSTEM PROJECT**
DESCHUTES MARKET RD / GREYSTONE LN INTERSECTION IMPROVEMENT

Graystone Lane / Deschutes Pleasant Ridge Road Intersection Improvement



Graystone Lane is a collector roadway which provides access to northbound US 97 from Deschutes Market Road. Deschutes Pleasant Ridge Road connects rural properties east of US 97 to highway access points and Deschutes Market Road. The Graystone Lane/Deschutes Pleasant Ridge intersection presently does not adequately accommodate predominant traffic movements and lacks appropriate sight distance.

- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
- Road Name: Graystone Lane Deschutes Pleasant Ridge Road
- Functional Classification: Rural Collector Rural Collector
- ADT: Not Available 4,149 (2022)

SCOPE OF WORK

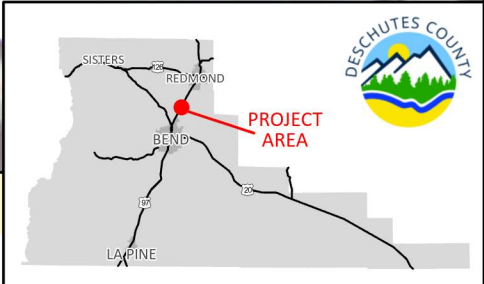
- Implementing all-way stop control at the intersection
- Installing new Signage, Delineation and Pavement Markings
- Removing roadside obstructions to improve sight distance

FUNDING

	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$300,000	\$100,000	-	\$400,000
RIGHT OF WAY	-	\$200,000	-	\$200,000
CONSTRUCTION	-	\$500,000	\$2,000,000	\$2,500,000
TOTAL	\$300,000	\$800,000	\$2,000,000	\$3,100,000

SCHEDULE

	<i>FY 2026</i>				<i>FY 2027</i>				<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2028

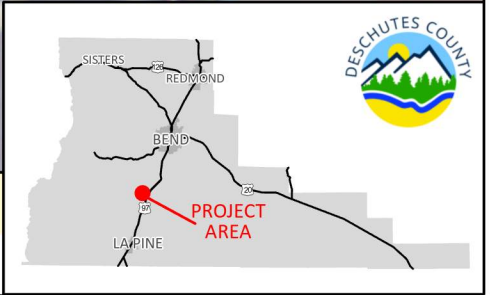
TRANSPORTATION SYSTEM PROJECT
GREYSTONE LN / PLEASANT RIDGE INTERSECTION IMPROVEMENT



 Project Extents

2028

TRANSPORTATION SYSTEM PROJECT
S. CENTURY DR / VENTURE LN INTERSECTION IMPROVEMENT



NW Lower Bridge Way: NW 43rd Street to NW Teater Avenue

NW Lower Bridge Way is an east-west rural collector connecting the area west of Terrebonne to US 97. It is a primary route for both recreational and residential traffic, and is part of the Sisters to Smith Rock Scenic Bikeway. The section of NW Lower Bridge Way from NW 43rd Street to Teater Avenue has a paved roadway width of 24 feet and a pavement condition index (PCI) ranging from 74 to 77 out of 100. The roadway currently lacks dedicated bicycle facilities and does not meet current collector roadway standards.



- Project Justification: Deschutes County TSP 2010 – 2030 (Medium Priority)
- Road Name: NW Lower Bridge Way NW Teater Avenue
- Functional Classification: Rural Collector Rural Local
- ADT: 1,129 (2022) Not Available

SCOPE OF WORK

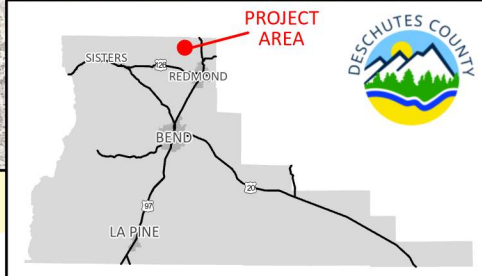
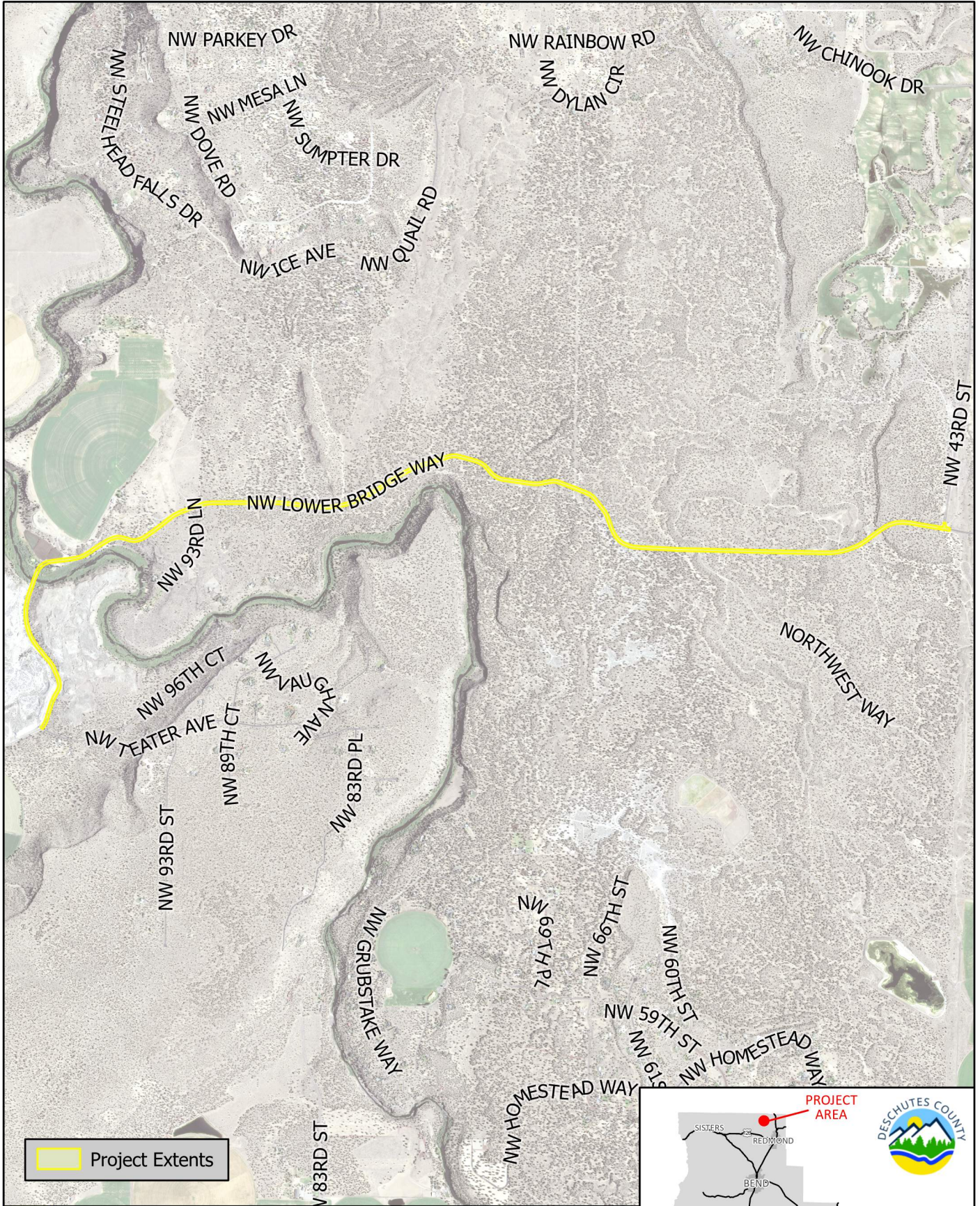
- Widening of NW Lower Bridge Way to a paved width of 28 feet with 2-foot aggregate shoulders to accommodate paved bikeways
- Paving of NW Lower Bridge Way between NW 43rd Street and Teater Avenue
- Safety improvements – Signing, Guardrail, Striping and Delineation
- Removal of roadside hazards

FUNDING

	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$300,000	\$150,000	\$450,000
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$1,800,000	\$1,800,000
TOTAL	\$300,000	\$1,950,000	\$2,250,000

SCHEDULE

	<i>FY 2027</i>				<i>FY 2028</i>				<i>FY 2029</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2028

TRANSPORTATION SYSTEM PROJECT

NW LOWER BRIDGE WAY: NW 43RD ST TO TEATER AVE

**Paving of Hamby Road:
US 20 to Butler Market Road**

Hamby Road is an north-south arterial located east of Bend that begins at US 20 and ends at Butler Market Road. It is critical link for communities east of Bend to access US 20 and the City of Bend, and is increasingly used as an eastside bypass of Bend by traffic bound for US 97 via Butler Mkt Road, Hamhook Road, and Deshutes Mkt Road. It also provides access to Buckingham Elementary School, and bisects three Deschutes County Bikeways.



- Project Justification: Pavement Condition Index (PCI) Rating – 79 out of 100
- Functional Classification: Rural Arterial
- ADT: 3,447 (2022)

SCOPE OF WORK

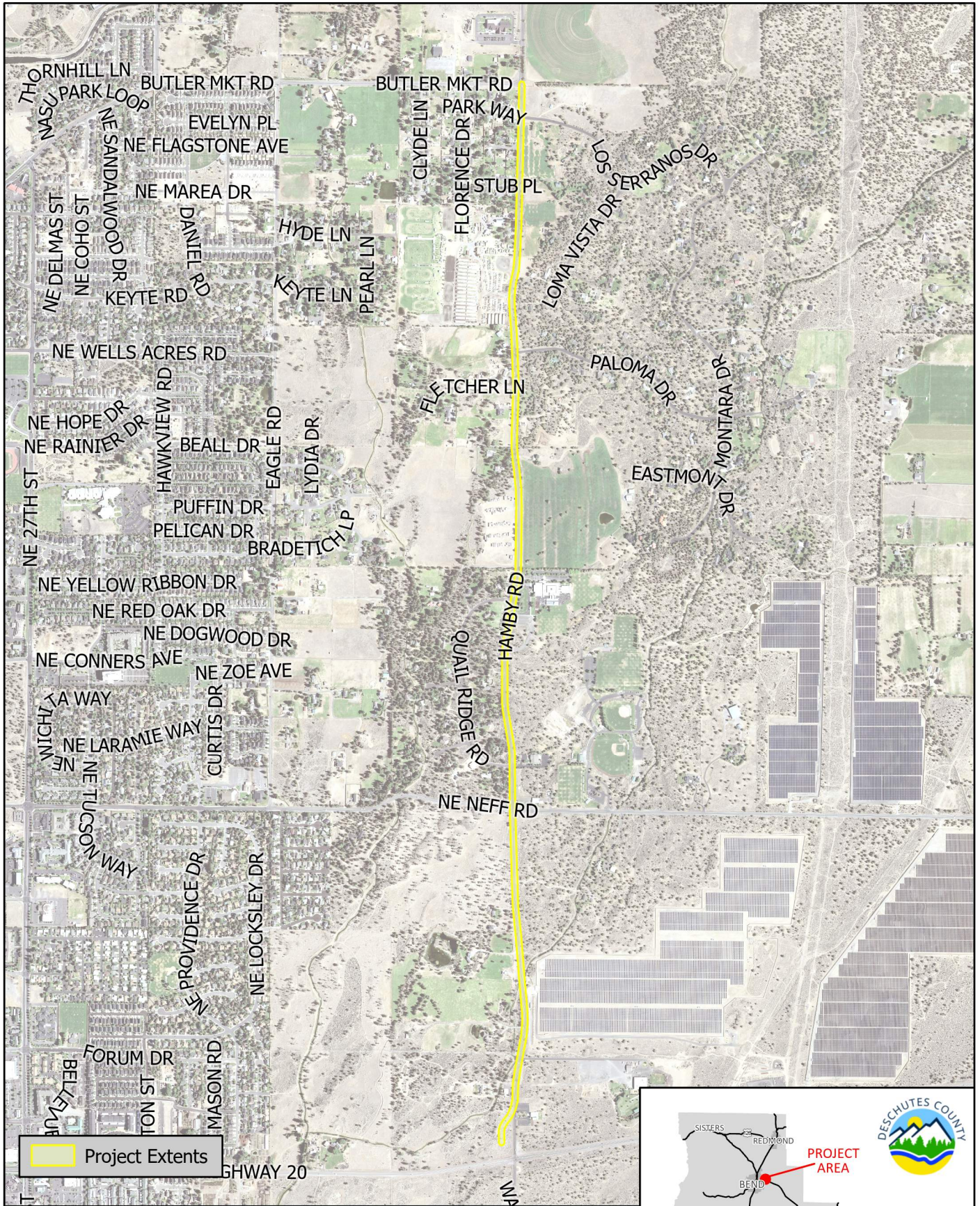
- Asphalt pavement rehabilitation via overlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2023</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$999,285	\$999,285
TOTAL	\$999,285	\$999,285

SCHEDULE

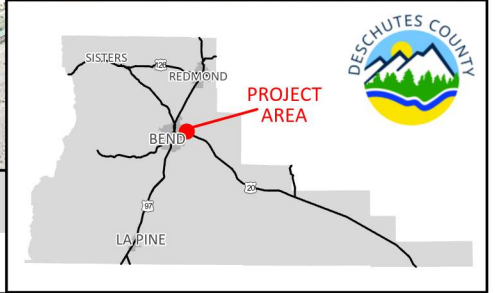
	<i>FY 2023</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2023

PAVEMENT PRESERVATION PROJECT

HAMBY RD: US 20 TO BUTLER MARKET RD



**Paving of Alfalfa Market Road:
M.P. 4 to Johnson Ranch Road /
Willard Road**



Alfalfa Market Road is an east-west rural arterial beginning at Powell Butte Hwy and ending 9.45 miles to the east at a 4 way intersection with Johnson Ranch Road, Willard Road and Walker Road. The road links east Bend to Prineville Reservoir and the Crooked River Highway. This moderately-trafficked rural roadway segment is experiencing significant pavement distress and shoulder deterioration, and lacks adequate delineation.

- Project Justification: Pavement Condition Index (PCI) Rating – 75 out of 100
- Functional Classification: Rural Arterial
- ADT: 1,596 (2022)

SCOPE OF WORK

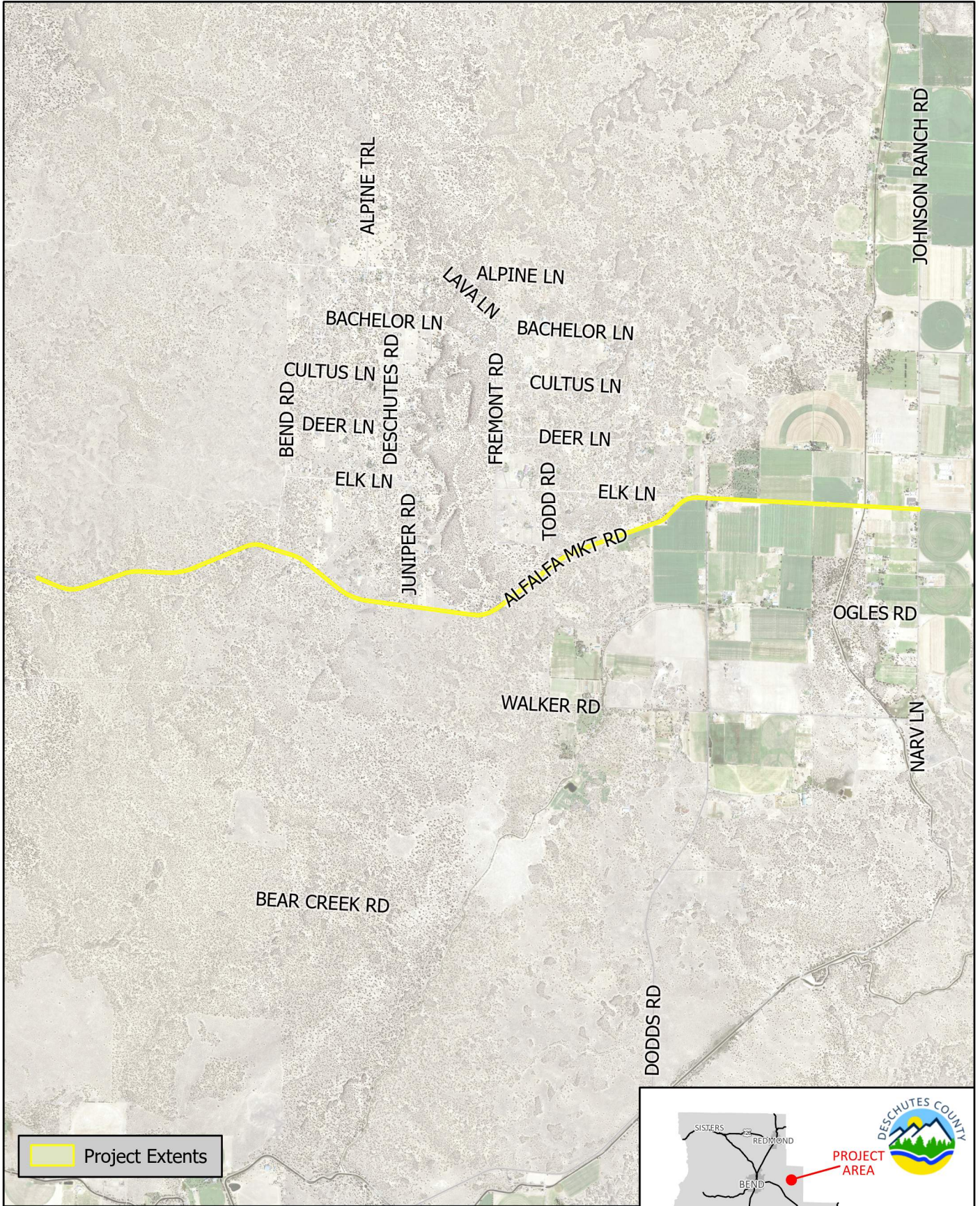
- Asphalt pavement rehabilitation via overlay
- Shoulder repair & full-depth pavement repair
- Minor improvements – Striping and Delineation

FUNDING

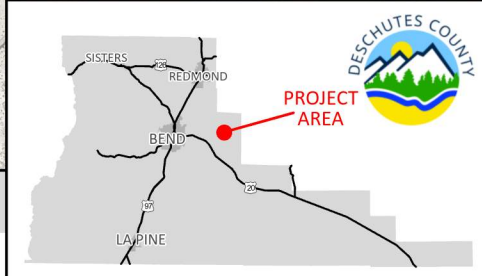
	<i>FY 2023</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$1,788,826	\$1,788,826
TOTAL	\$1,788,826	\$1,788,826

SCHEDULE

	<i>FY 2023</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



 Project Extents



2023

PAVEMENT PRESERVATION PROJECT
 ALFALFA MKT RD: MP 4 TO JOHNSON MKT RD / WILLARD RD

Paving of Rosland Road / Tracy Road: US 97 to Drafter Road

Rosland Road is a City collector roadway which connects the Newberry Estates community northeast of La Pine to US 97. The road also provides recreational users with access to Forest Service Road #2205 and the Deschutes National Forest. Wendy Road and Tracy Road are City local roadways that provide connectivity from Rosland Road to local businesses. These heavily-trafficked roadway segments are the primary ingress to a nearby truck stop and other local businesses. These roadways are exhibiting pavement failure in multiple locations due to high volumes of heavy vehicles.



- Project Justification: Pavement Condition Index (PCI) Rating – 48 to 79 out of 100
- Road Name: Rosland Road Wendy Road & Tracy Road
- Functional Classification: City Collector City Local
- ADT: 1,064 (2022) Not Available

SCOPE OF WORK

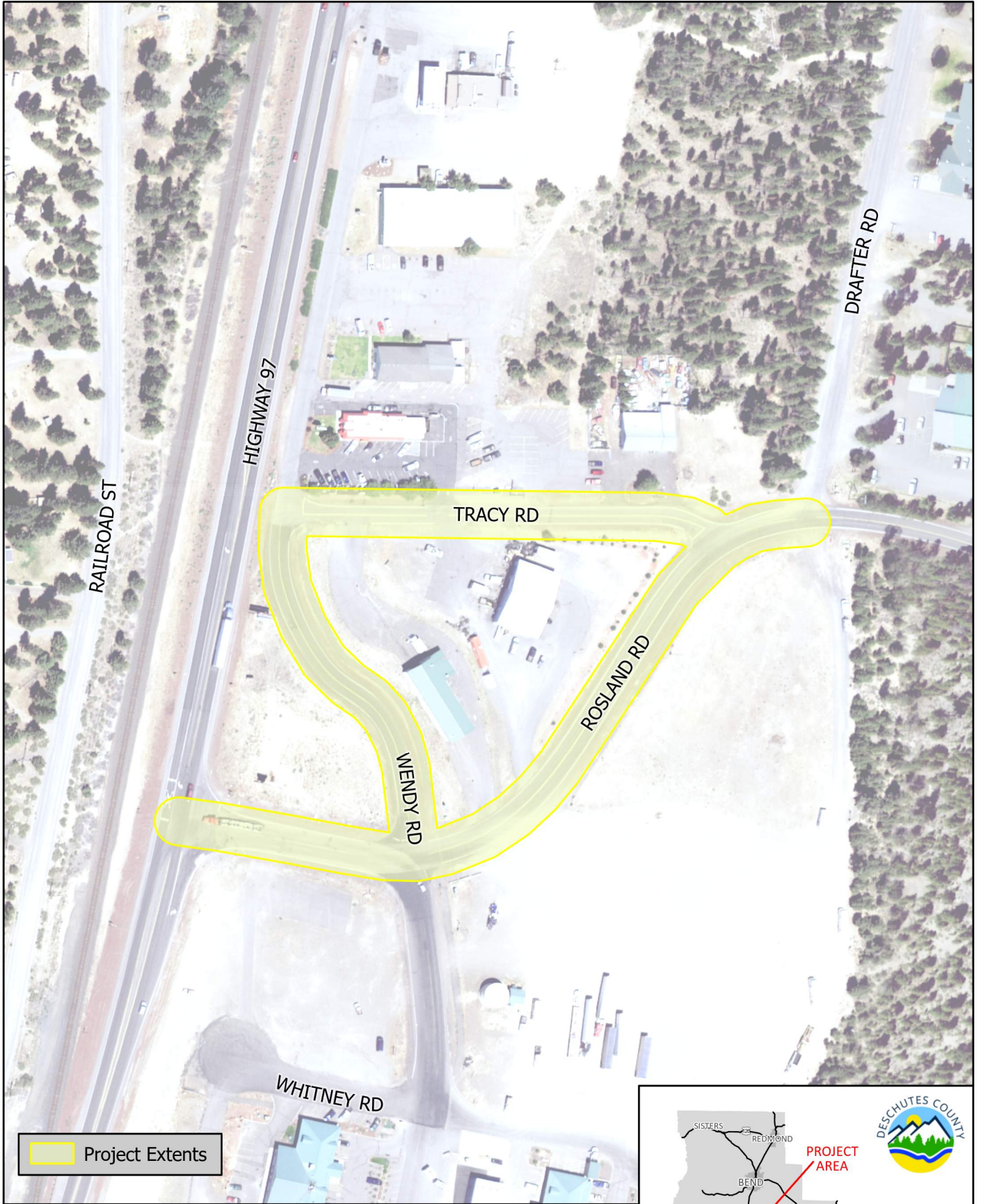
- Asphalt pavement reconstruction
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$393,000	\$250,673	\$643,673
TOTAL	\$393,000	\$250,673	\$643,673

SCHEDULE

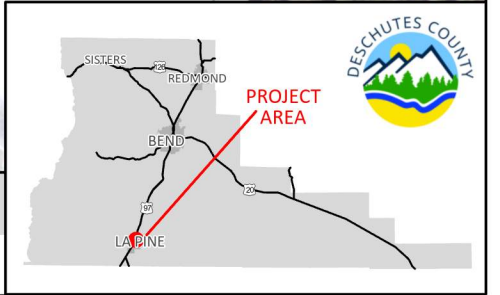
	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



 Project Extents

2023

PAVEMENT PRESERVATION PROJECT
 ROSLAND RD / TRACY RD: US 97 TO DRAFTER RD



**Paving of Butler Market Road:
Homehook Road to Powell Butte
Hwy**

Butler Market Road is an east-west arterial roadway connecting east Bend to Powell Butte Highway and the Bend Municipal Airport. Butler Market Road exhibits pavement deterioration which warrants rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 84 out of 100
- Functional Classification: Rural Arterial
- ADT: 4,897 (2022)

SCOPE OF WORK

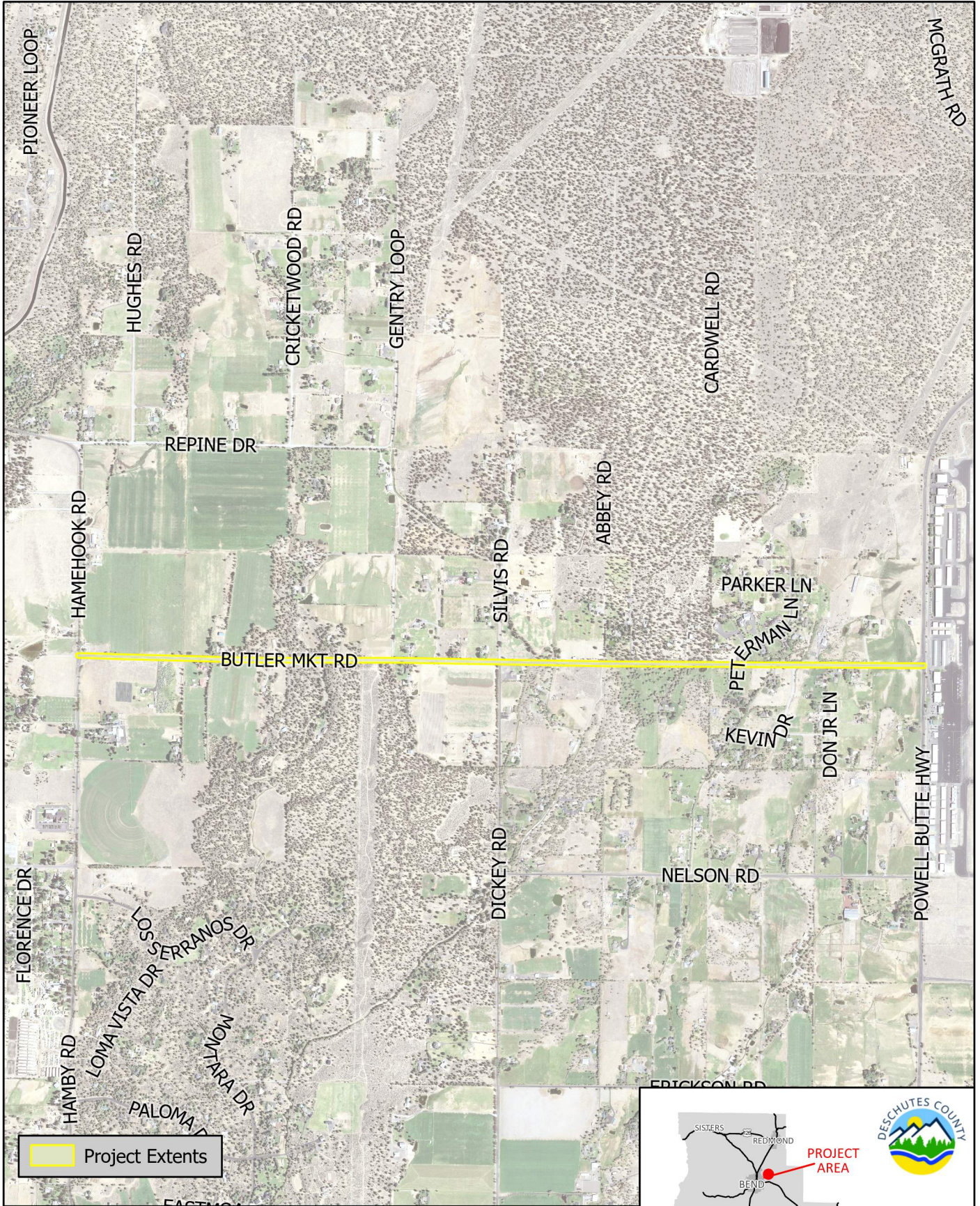
- Asphalt pavement rehabilitation via inlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$1,000,000	\$320,000	\$1,320,000
TOTAL	\$1,000,000	\$320,000	\$1,320,000

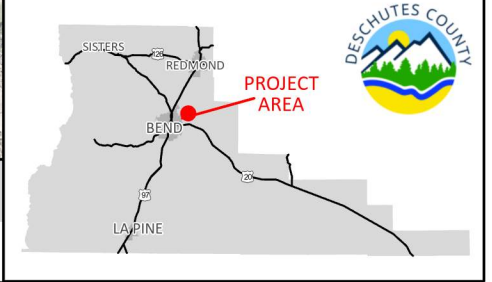
SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2023

PAVEMENT PRESERVATION PROJECT
 BUTLER MARKET RD: HAMEHOOK RD TO POWELL BUTTE HWY



Paving of Dickey Road

Dickey Road is a north-south collector roadway east of Bend that carries traffic between Butler Market Road and Erickson Road. The roadway connects residents of the rural area to US 20 and Bend. Dickey Road exhibits pavement deterioration and has roadside hazards that require removal.



- Project Justification: Pavement Condition Index (PCI) Rating – 74 out of 100
- Functional Classification: Rural Collector
- ADT: 852 (2022)

SCOPE OF WORK

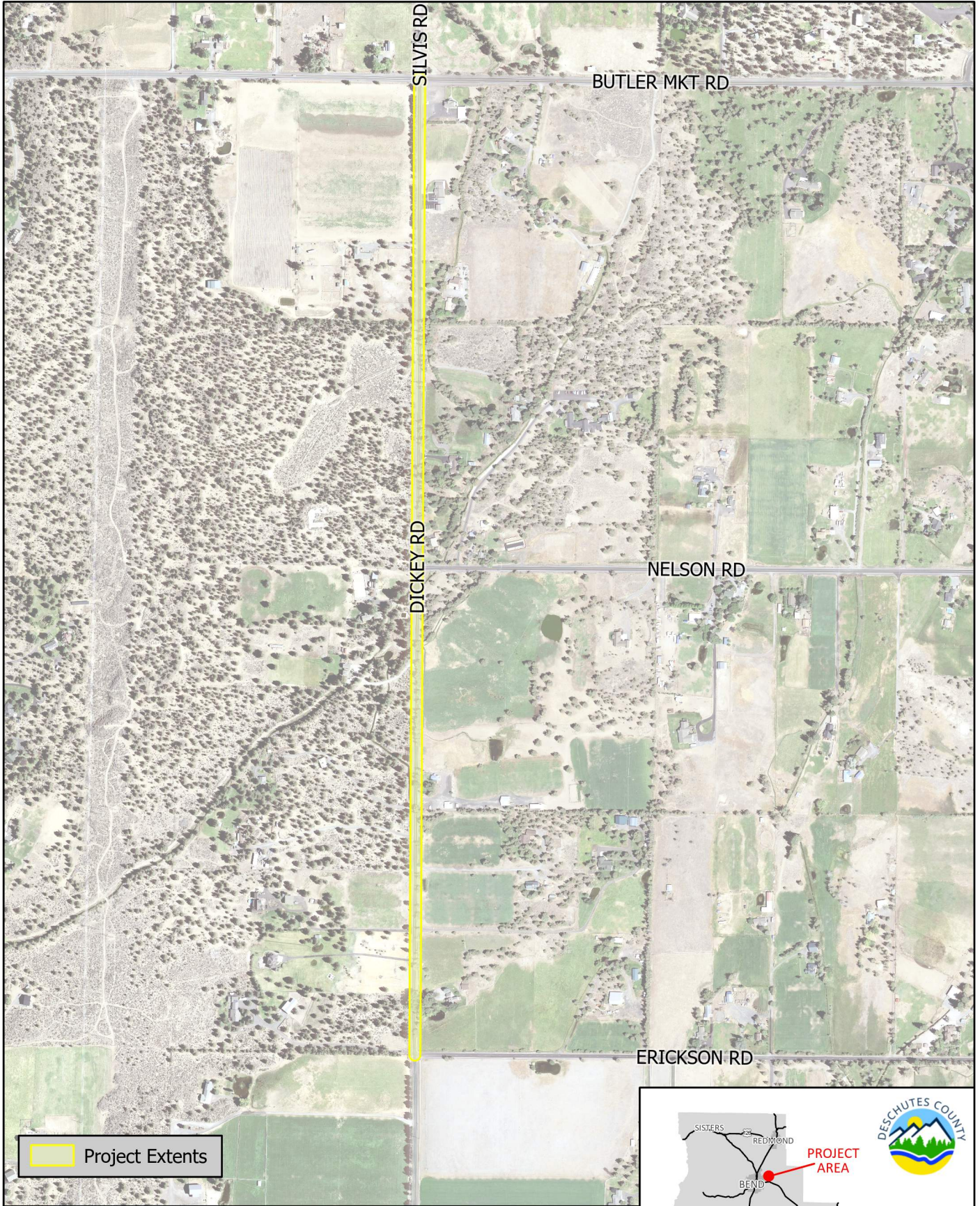
- Asphalt pavement rehabilitation via overlay
- Isolated full-depth pavement and shoulder repair
- Minor improvements – Striping and Delineation
- Removal of roadside obstructions

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$250,000	\$600,000	\$850,000
TOTAL	\$250,000	\$600,000	\$850,000

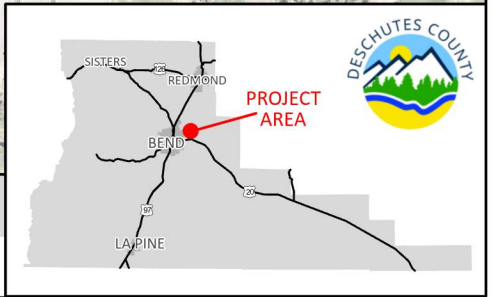
SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2024

PAVEMENT PRESERVATION PROJECT
 DICKEY RD



**Paving of Old Bend-Redmond Hwy:
US 20 to Tumalo Road**

Old Bend-Redmond Hwy is a north-south arterial roadway which links communities in the Tumalo and South Redmond areas to US 20 and North Bend. Old Bend Redmond Hwy exhibits pavement deterioration which warrants rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 85 out of 100
- Functional Classification: Rural Arterial
- ADT: 4,525 (2022)

SCOPE OF WORK

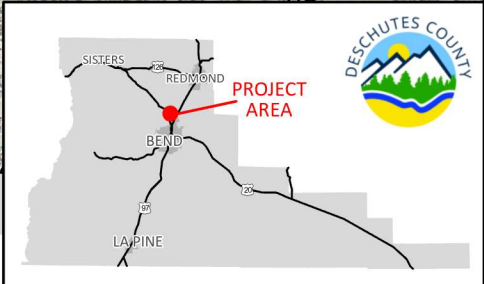
- Asphalt pavement rehabilitation via inlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$500,000	\$1,210,000	\$1,721,000
TOTAL	\$500,000	\$1,210,000	\$1,721,000

SCHEDULE

	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2024

PAVEMENT PRESERVATION PROJECT

OLD BEND REDMOND HWY: US20 TO TUMALO RD

Paving of Horse Butte Road

Horse Butte Road is a north-south rural local roadway located southeast of Bend. It serves as the primary access for residents of the Sundance East subdivision, and also provides recreational access to the Deschutes National Forest. The roadway exhibits significant pavement deterioration and poor ride quality. The segment also has several roadside safety hazards which require removal.



- Project Justification: Pavement Condition Index (PCI) Rating – 61 out of 100
- Functional Classification: Rural Local
- ADT: 929 (2022)

SCOPE OF WORK

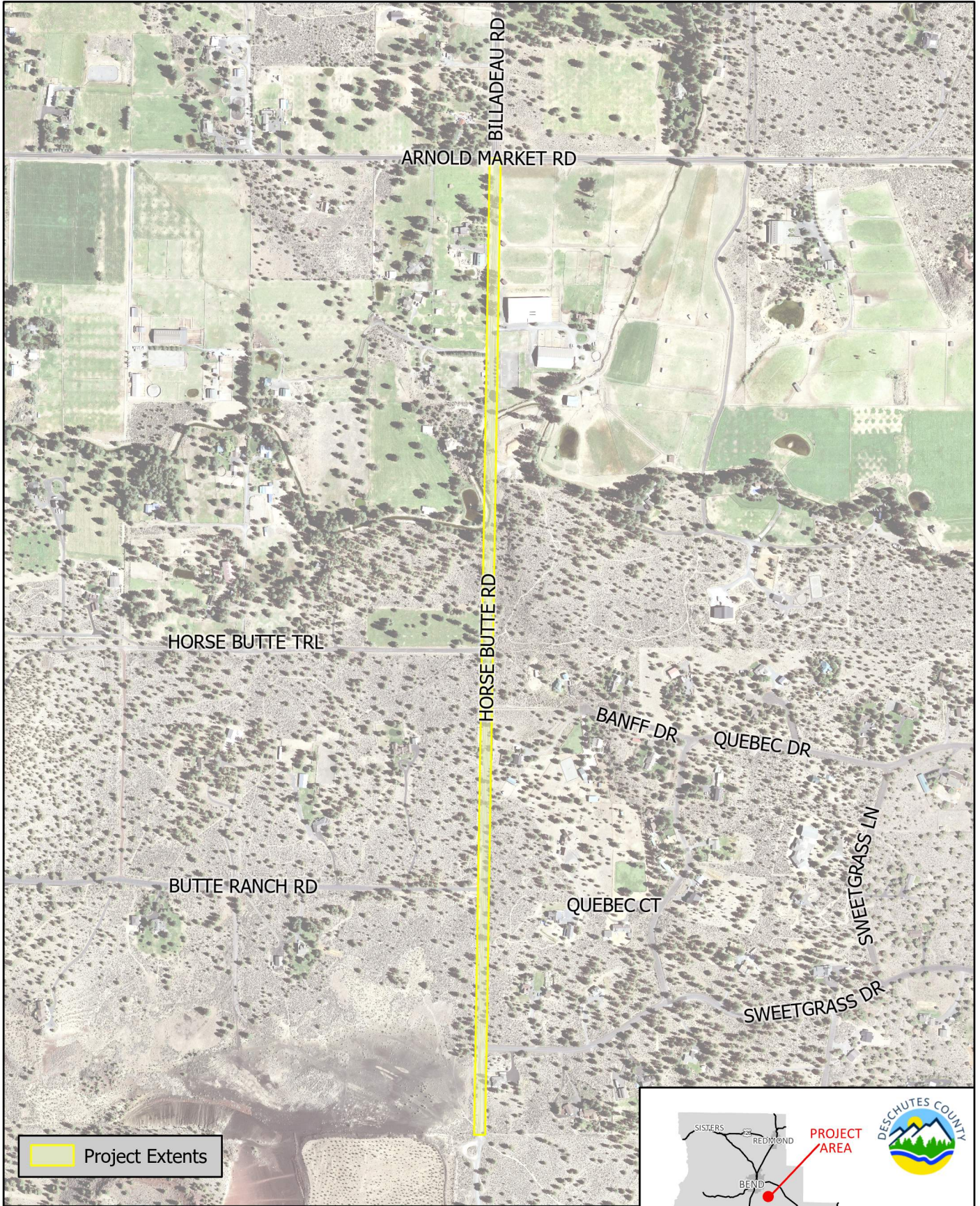
- Asphalt pavement rehabilitation via inlay/overlay
- Isolated full-depth pavement repair
- Minor improvements – Striping and Delineation
- Removal of roadside hazards

FUNDING

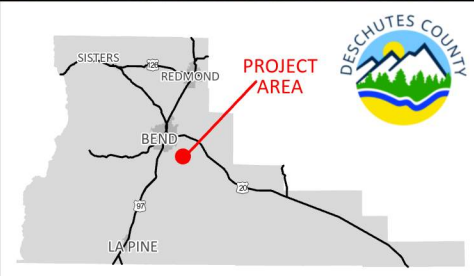
	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$460,000	\$460,000
TOTAL	\$460,000	\$460,000

SCHEDULE

	<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



Project Extents



2024

PAVEMENT PRESERVATION PROJECT

HORSE BUTTE RD

Paving of Old Bend-Redmond Hwy / S Canal Boulevard: Tumalo Road to Helmholtz Way

Old Bend-Redmond Hwy is a north-south arterial roadway which links communities in the Tumalo and South Redmond areas to US 20 and North Bend. S Canal Boulevard is also a north-south arterial which provides access for communities south of Redmond to Tumalo. Both roadways exhibit pavement deterioration which warrants rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 85 out of 100
- Road Name: Old Bend-Redmond Hwy S Canal Boulevard
- Functional Classification: Rural Arterial Rural Arterial
- ADT: 4,971 (2022) 8,246 (2022)

SCOPE OF WORK

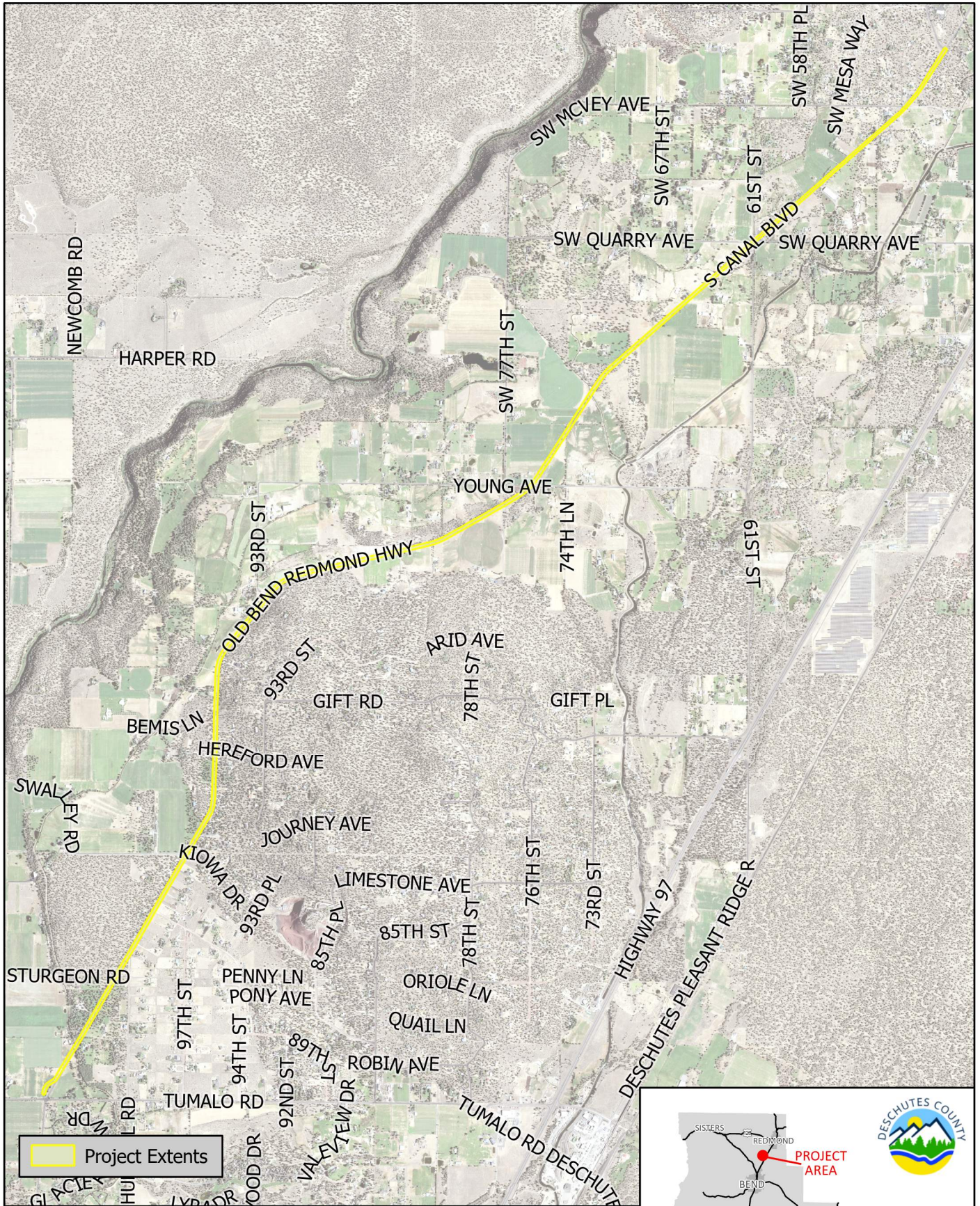
- Asphalt pavement rehabilitation via inlay
- Minor improvements – Striping and Delineation


FUNDING

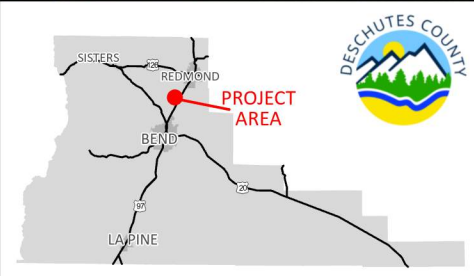
	<i>FY 2024</i>	<i>FY 2025</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$3,000,000	\$410,000	\$3,410,000
TOTAL	\$3,000,000	\$410,000	\$3,410,000

SCHEDULE

	<i>FY 2024</i>				<i>FY 2025</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



 Project Extents



2024

PAVEMENT PRESERVATION PROJECT

S. CANAL / OLD BEND REDMOND HWY: TUMALO RD TO HELMHOLTZ WAY

**Paving of Spring River Road:
S Century Drive to Stellar Drive**

Spring River Road is located in south Sunriver and connects traffic from Sunriver Resort and US 97 to communities west of Sunriver and the Deschutes National Forest. The segment is classified as a rural arterial. A popular access point to the Deschutes River exists along the Project Segment, which brings significant seasonal traffic during the summer months. New development in the area has also resulted in increased traffic levels. The existing pavement is exhibiting considerable load-related distress and maintenance patching.



- Project Justification: Pavement Condition Index (PCI) Rating – 81 out of 100
- Functional Classification: Rural Arterial
- ADT: 5,599 (2022)

SCOPE OF WORK

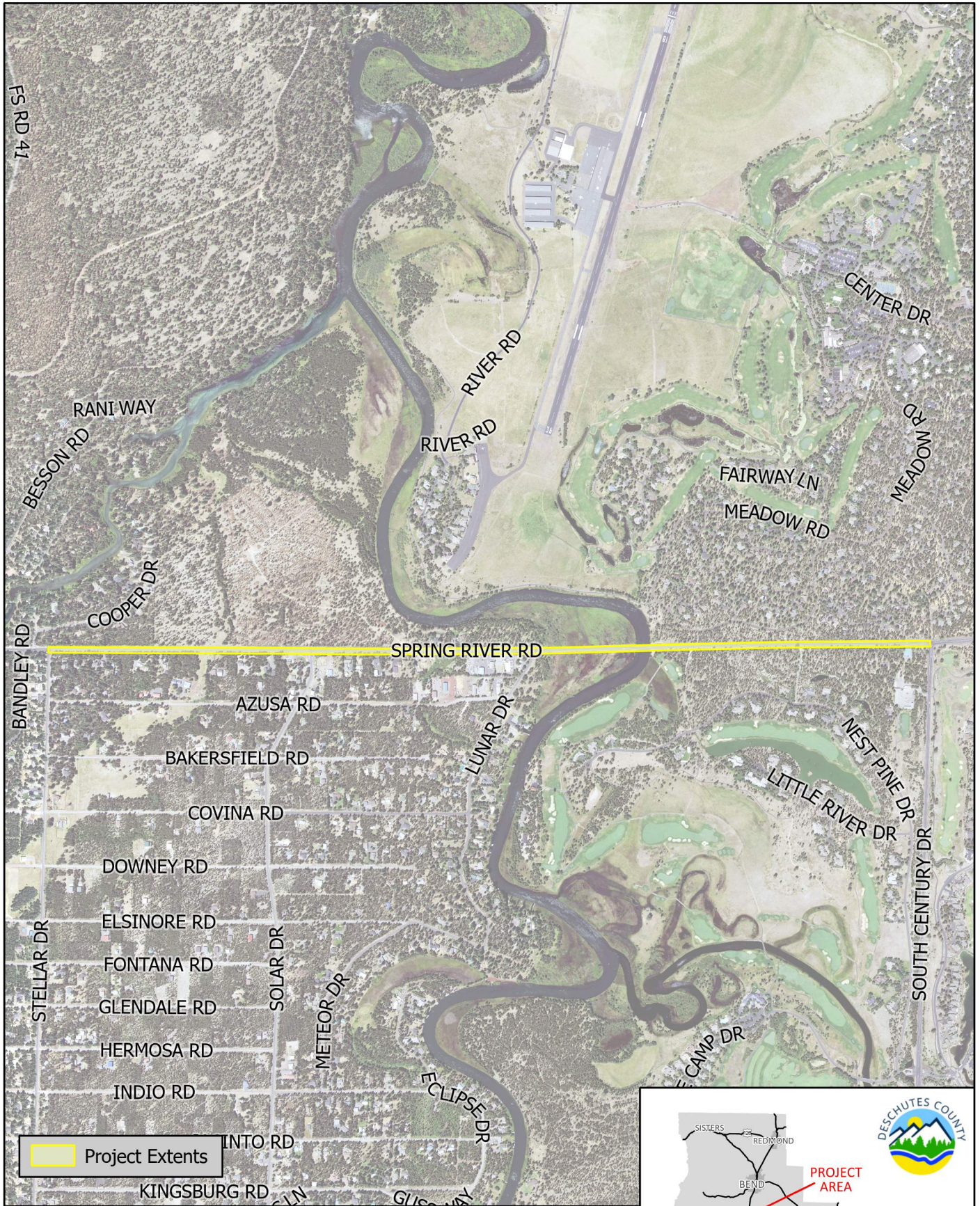
- Rehabilitation of existing asphalt pavement via overlay/inlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2024</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$510,000	\$500,000	\$1,010,000
TOTAL	\$510,000	\$500,000	\$1,010,000

SCHEDULE

	<i>FY 2024</i>				<i>FY 2025</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								

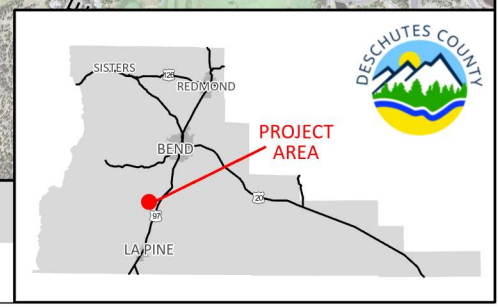


 Project Extents

2024

PAVEMENT PRESERVATION PROJECT

SPRING RIVER RD: S. CENTURY DR TO STELLAR DR



**Paving of Powell Butte Highway:
McGrath Road to US 20**

Powell Butte Highway is a rural arterial roadway located east of Bend. The roadway connects communities east of Bend to US 20, Crook County and the Bend Municipal Airport. This segment is frequented by road users commuting from outlying rural communities to the City of Bend, and is also a popular freight route. The pavement is exhibiting load-related cracking and other pavement distress which warrants rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 80 out of 100
- Functional Classification: Rural Arterial
- ADT: 7,418 (2022)

SCOPE OF WORK

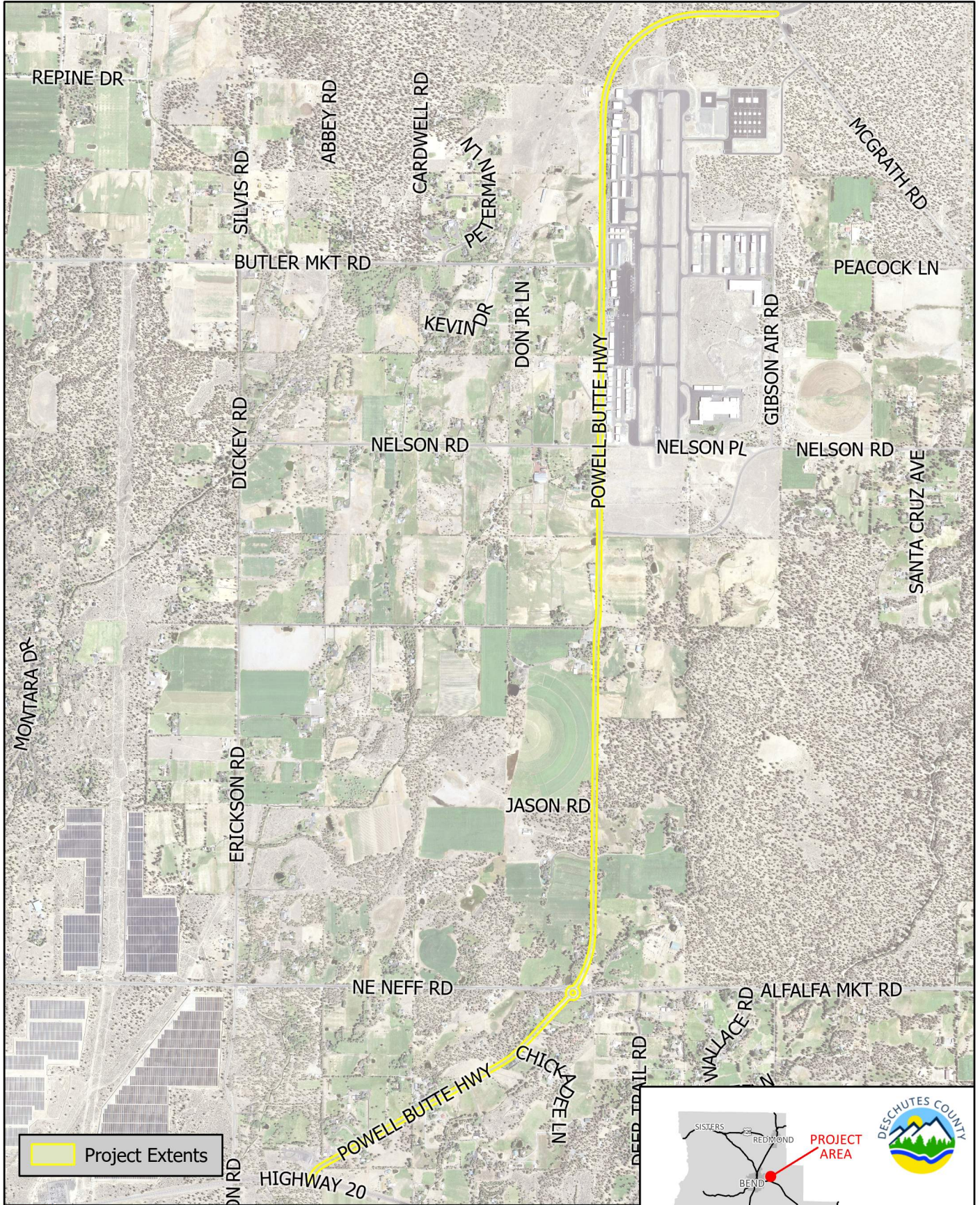
- Rehabilitation of existing asphalt pavement via overlay/inlay
- Paving of roundabout and roundabout legs at NE Neff Road/Alfalfa Market Road
- Minor improvements – Striping and Delineation

FUNDING

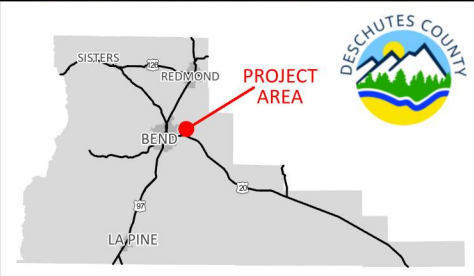
	<i>FY 2025</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$2,290,000	\$2,290,000
TOTAL	\$2,290,000	\$2,290,000

SCHEDULE

	<i>FY 2025</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



Project Extents



2025

PAVEMENT PRESERVATION PROJECT

POWELL BUTTE HWY: MCGRATH RD TO US 20

Paving of Northwest Way: NW Coyner Avenue to NW Almeter Way



Northwest Way is a north-south rural collector which serves communities northwest of Redmond. This roadway is a critical link for these rural communities, providing access to the City of Redmond. The segment is experiencing pavement deterioration and lacks modern safety features such as roadway delineation.

- Project Justification: Pavement Condition Index (PCI) Rating – 80 out of 100
- Functional Classification: Rural Collector
- ADT: 2,536 (2022)

SCOPE OF WORK

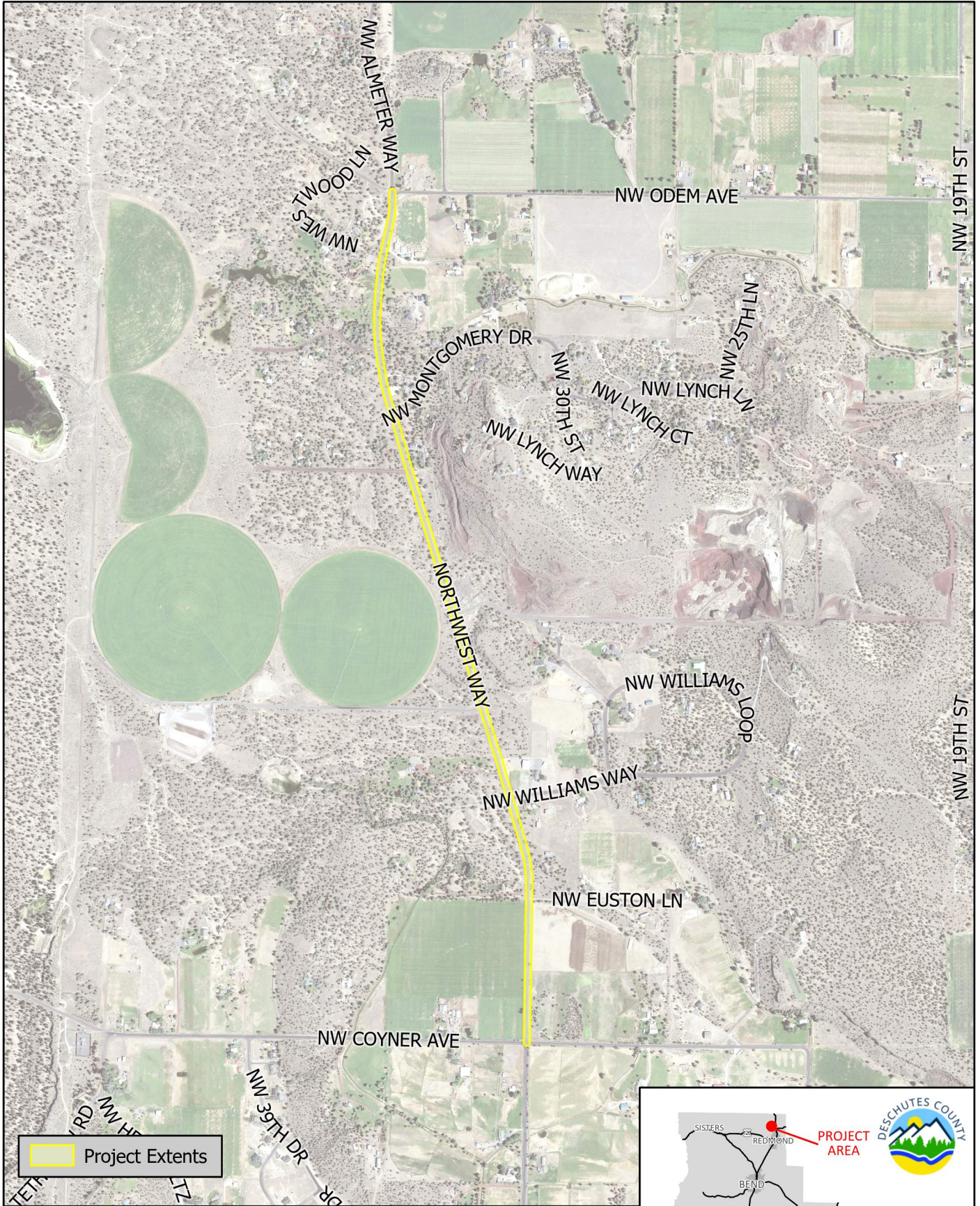
- Asphalt pavement rehabilitation via inlay/overlay
- Minor improvements – Striping and Delineation

FUNDING

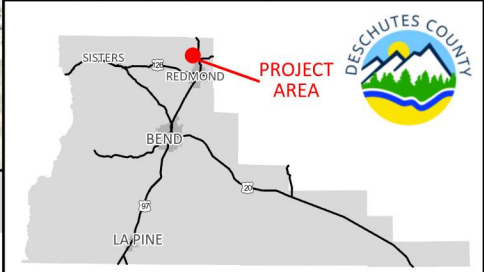
	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$815,000	\$556,000	\$1,371,000
TOTAL	\$815,025	\$556,026	\$1,371,000

SCHEDULE

	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



 Project Extents



2025

PAVEMENT PRESERVATION PROJECT
 NORTHWEST WAY: NW COYNER AVE TO NW ALTMETER WAY

Paving of Billadeau Road

Billadeau Road is a north-south collector roadway east of Bend that carries traffic between Ward Road and Horse Butte Road. The roadway links rural communities east of Bend to US 20 and the Bend city limits. Billadeau Road exhibits pavement deterioration and ride quality issues which warrant rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 77 out of 100
- Functional Classification: Rural Collector

SCOPE OF WORK

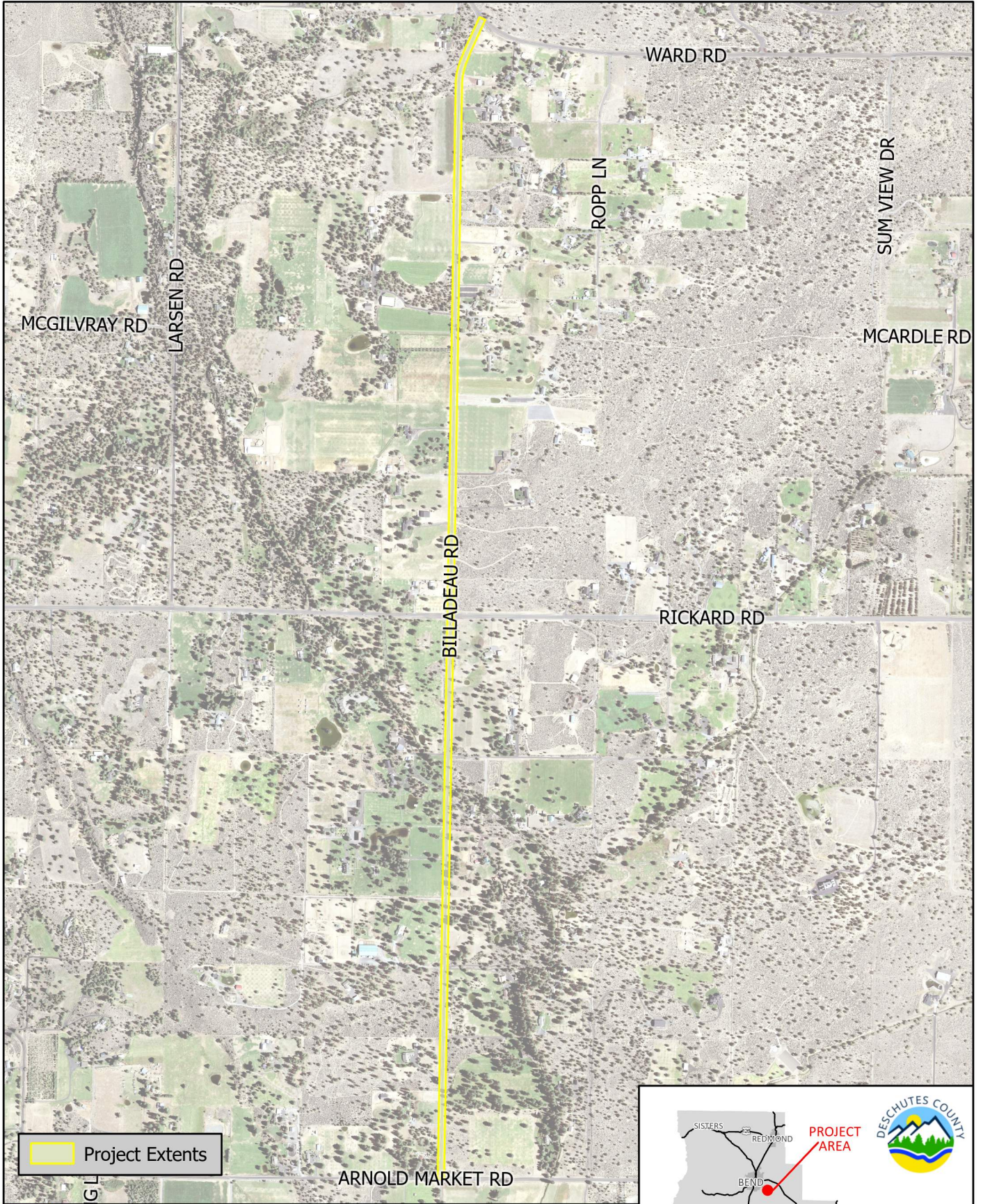
- Asphalt pavement rehabilitation via inlay/overlay
- Isolated full-depth pavement and shoulder repair
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$580,000	\$500,000	\$1,080,000
TOTAL	\$580,000	\$500,000	\$1,080,000

SCHEDULE

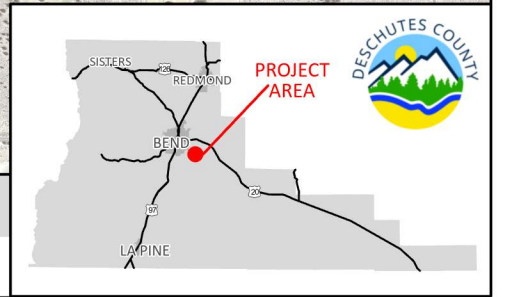
	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2025

PAVEMENT PRESERVATION PROJECT

BILADEAU RD



Paving of Skyline Ranch Road: Century Drive to City Limits

Skyline Ranch Road is an urban collector roadway located west of Bend. The roadway carries traffic from the Tetherow community to Century Drive and the Bend city limits. Skyline Ranch Road exhibits significant thermal and load-related pavement distress, warranting replacement of the existing asphalt pavement.



- Project Justification: Pavement Condition Index (PCI) Rating – 79 out of 100
- Functional Classification: Urban Collector
- ADT: 2,161 (2022)

SCOPE OF WORK

- Asphalt pavement rehabilitation via inlay/overlay
- Isolated full-depth pavement repair
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-	-
RIGHT OF WAY	-	-	-
CONSTRUCTION	\$500,000	\$1,320,000	\$1,820,000
TOTAL	\$500,000	\$1,320,000	\$1,820,000

SCHEDULE

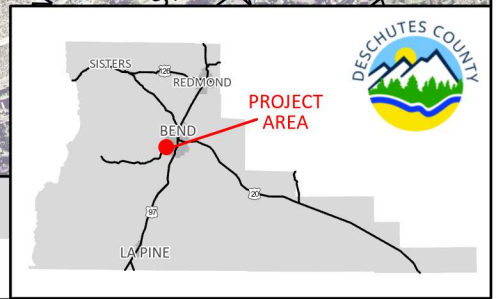
	<i>FY 2025</i>				<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2026

PAVEMENT PRESERVATION PROJECT

SKYLINE RANCH RD: CENTURY DR TO CITY LIMITS



Paving of Tumalo Road / Deschutes Market Road: 19th Street to Tumalo Place



Tumalo Road is an east-west rural collector and Deschutes Market Road is a north-south rural arterial. Both roadways provide access to US 97 north of Bend. This corridor has seen a significant increase in traffic due to Deschutes Market Road being frequently used as a bypass to communities east of Bend. Four County-maintained bridges also exist along the project segment, which are exhibiting isolated approach settlement warranting full-depth repair.

- Project Justification: Pavement Condition Index (PCI) Rating – 81 out of 100
Bridge Condition Reports
- Road Name: Tumalo Road Deschutes Market Road
- Functional Classification: Rural Collector Rural Arterial
- ADT: 7,973 (2022) 9,571 (2022)

SCOPE OF WORK

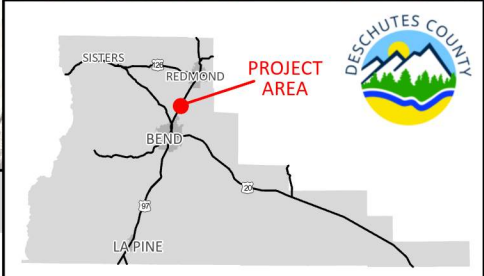
- Asphalt pavement rehabilitation via inlay/overlay
- Bridge approach full-depth repair
- Guardrail improvements
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$420,000	\$420,000
TOTAL	\$420,000	\$420,000

SCHEDULE

	<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2026

PAVEMENT PRESERVATION PROJECT

DESCHUTES MKT RD / TUMALO RD: 19TH ST TO TUMALO PL

Paving of Tumalo Road

Tumalo Road is an east-west rural collector. The roadway connects the community of Tumalo to US 97. The route is frequently used by commuter and truck traffic. Increased traffic levels and pavement deterioration warrant asphalt pavement rehabilitation.



- Project Justification: Pavement Condition Index (PCI) Rating – 81 out of 100
- Functional Classification: Rural Collector
- ADT: 4,477 (2022)

SCOPE OF WORK

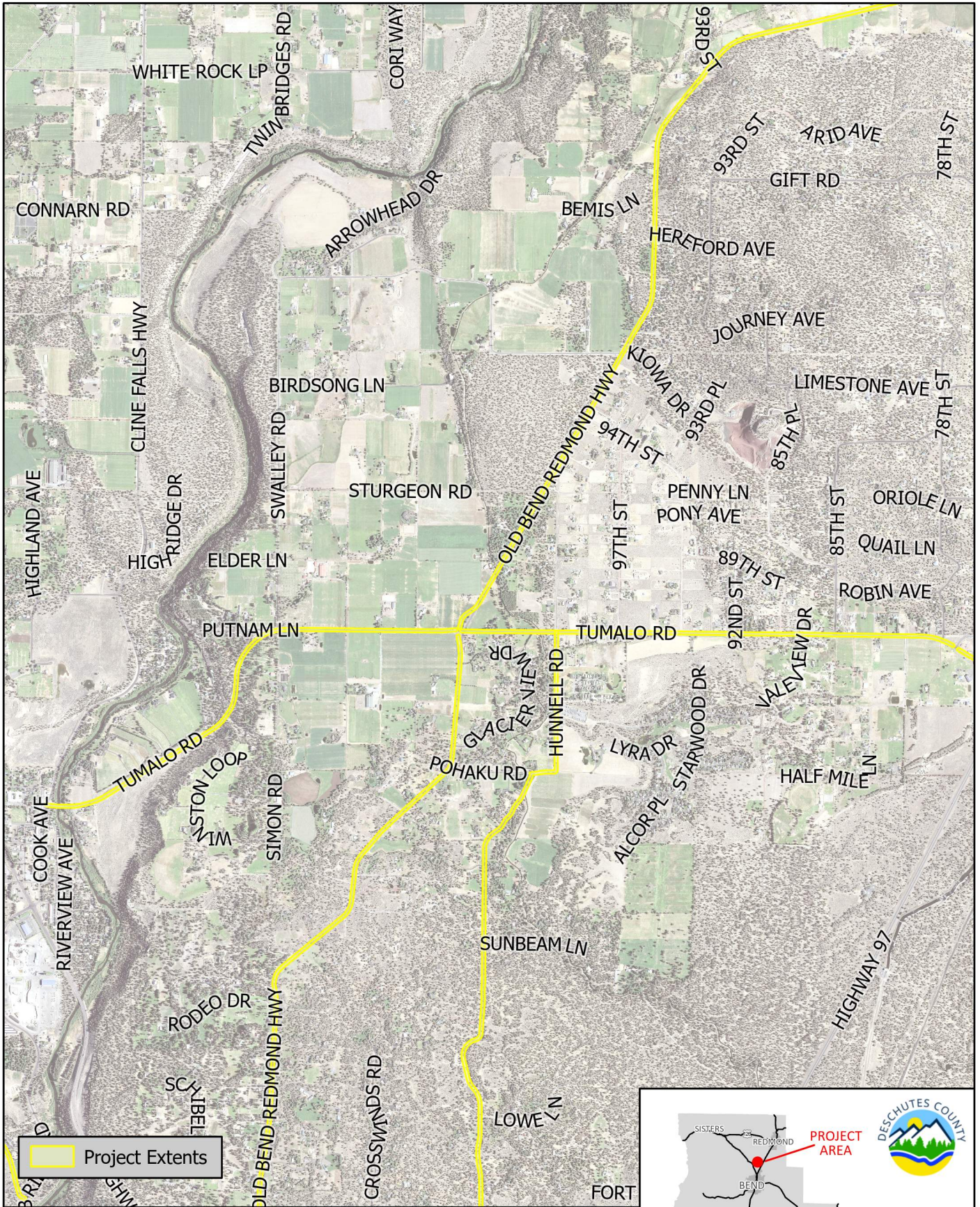
- Asphalt pavement rehabilitation via inlay/overlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$1,090,000	\$1,090,000
TOTAL	\$1,090,000	\$1,090,000

SCHEDULE

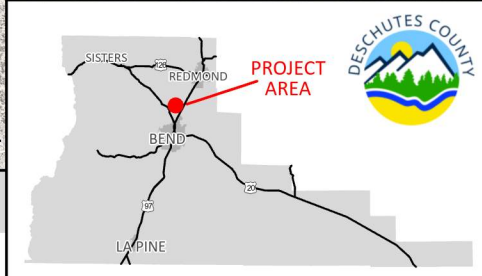
	<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2026

PAVEMENT PRESERVATION PROJECT

TUMALO RD



Paving of Dorrance Meadow Road

Dorrance Meadow Road is a north-south rural collector west of La Pine. The roadway links communities north of La Pine to the south La Pine city limits. The route also provides recreational access to the Deschutes River. Asphalt pavement rehabilitation is warranted due to thermal and load-related pavement distress.



- Project Justification: Pavement Condition Index (PCI) Rating – 83 out of 100
- Functional Classification: Rural Collector
- ADT: 1,188 (2022)

SCOPE OF WORK

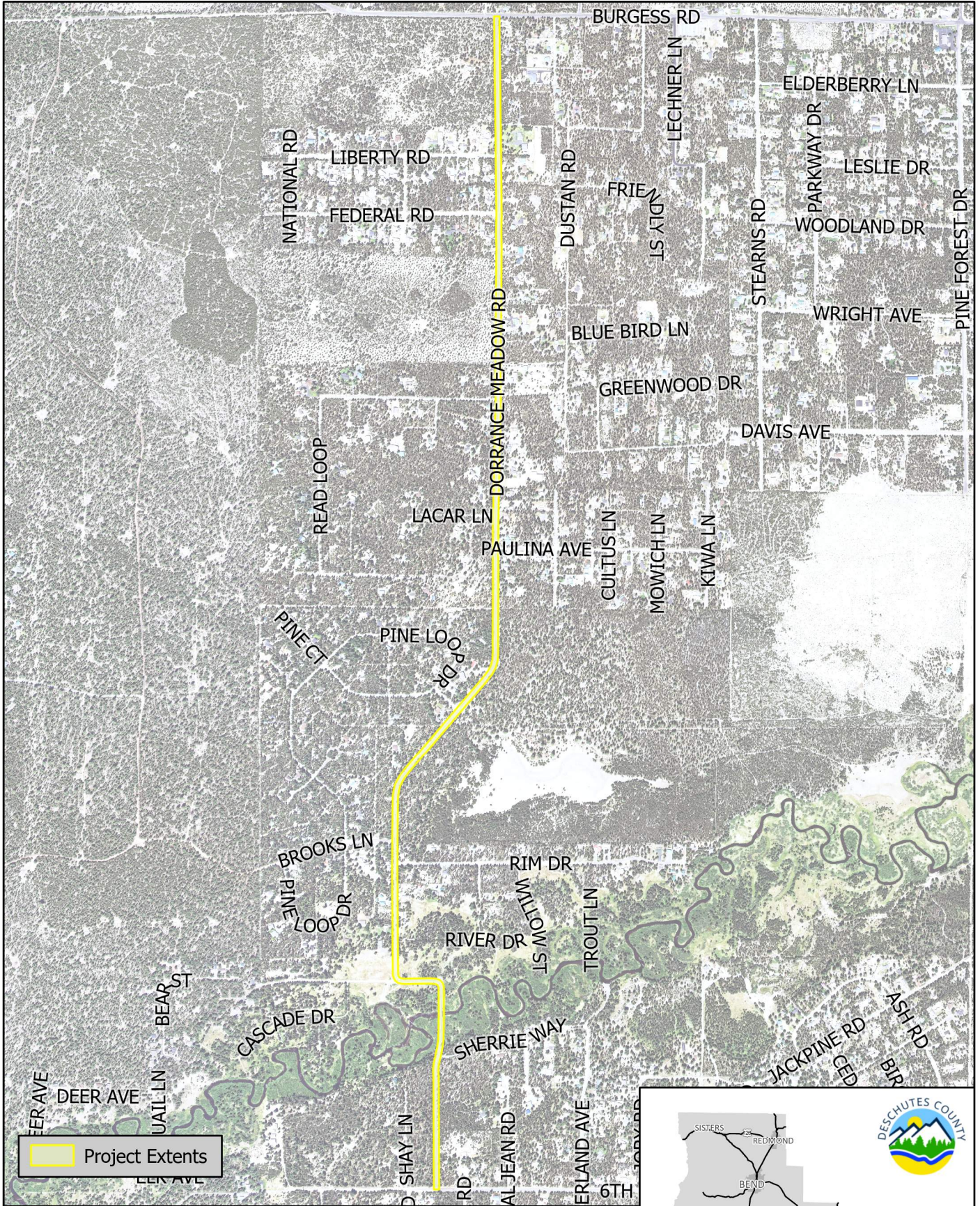
- Asphalt pavement rehabilitation via inlay/overlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2026</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$1,420,000	\$1,420,000
TOTAL	\$1,420,000	\$1,420,000

SCHEDULE

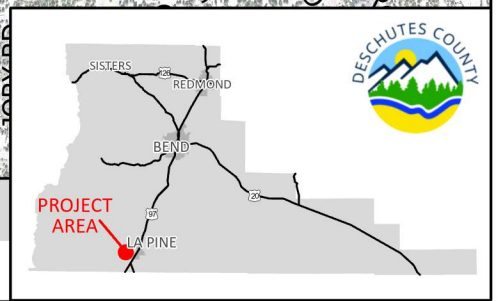
	<i>FY 2026</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2026

PAVEMENT PRESERVATION PROJECT

DORRANCE MEADOW RD



Paving of Indian Ford Road

Indian Ford Road is a rural collector roadway located north of the City of Sisters. The roadway connects rural communities to the City of Sisters and provides recreational access to the Deschutes National Forest. Asphalt pavement rehabilitation is warranted due to pavement distress and poor ride quality.



- Project Justification: Pavement Condition Index (PCI) Rating – 78 out of 100
- Functional Classification: Rural Collector
- ADT: 523 (2022)

SCOPE OF WORK

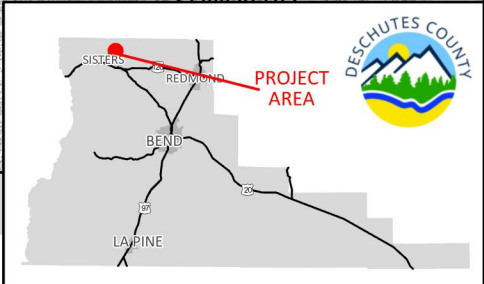
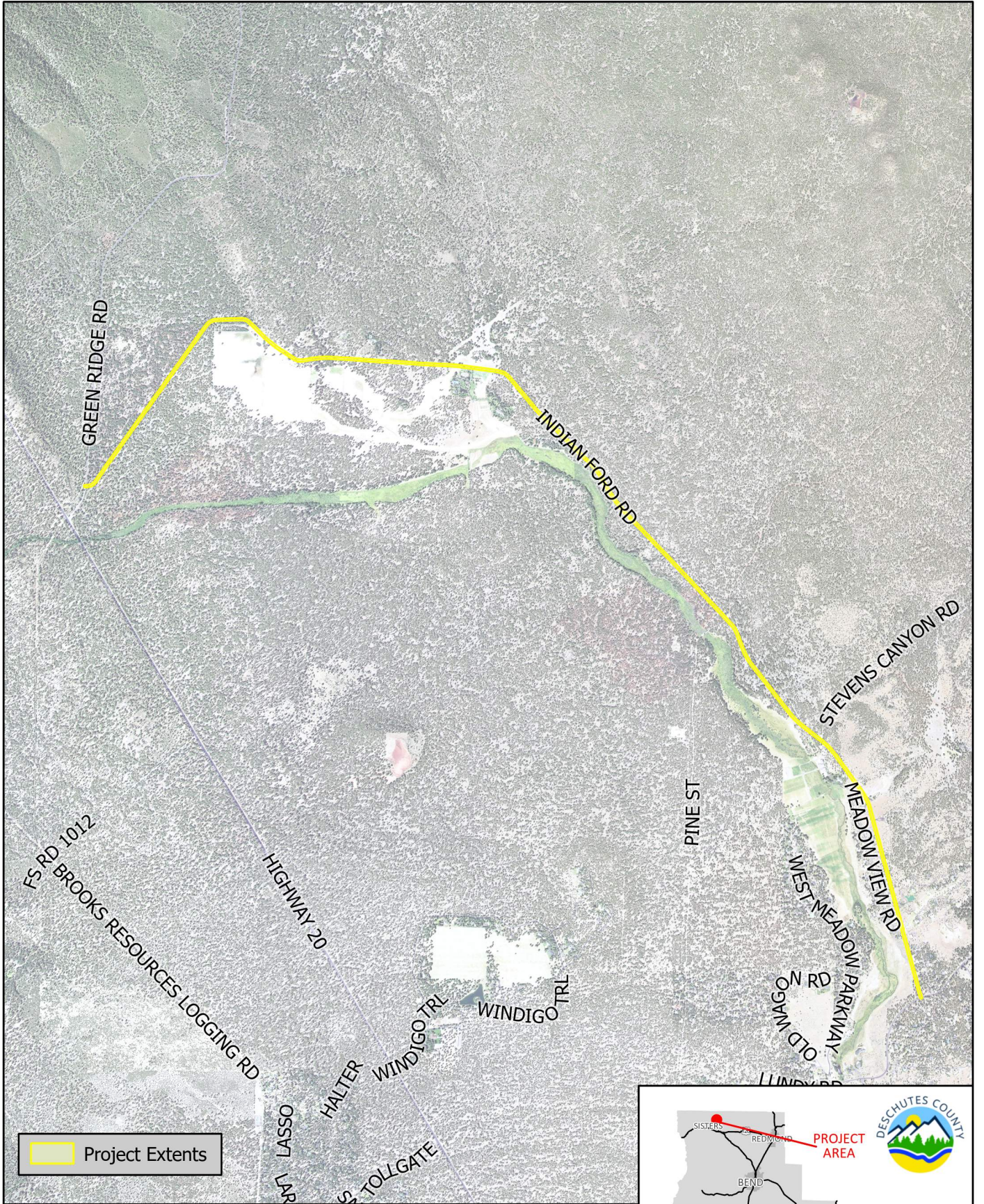
- Asphalt pavement rehabilitation via inlay/overlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$2,750,000	\$2,750,000
TOTAL	\$2,750,000	\$2,750,000

SCHEDULE

	<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2027

PAVEMENT PRESERVATION PROJECT

INDIAN FORD RD

Paving of 3rd Street / Walker Street / Pengra Street / 5th Street (La Pine)

Third Street, Walker Street, Pengra Street and 5th Street are contiguous local roadways in the City of La Pine. This corridor connects central La Pine with the communities west of City limits. These roadways exhibit significant load- and climate-related pavement distress.



- Project Justification: Pavement Condition Index (PCI) Rating – 73 to 79 out of 100
- Functional Classification: City Local (all streets)

SCOPE OF WORK

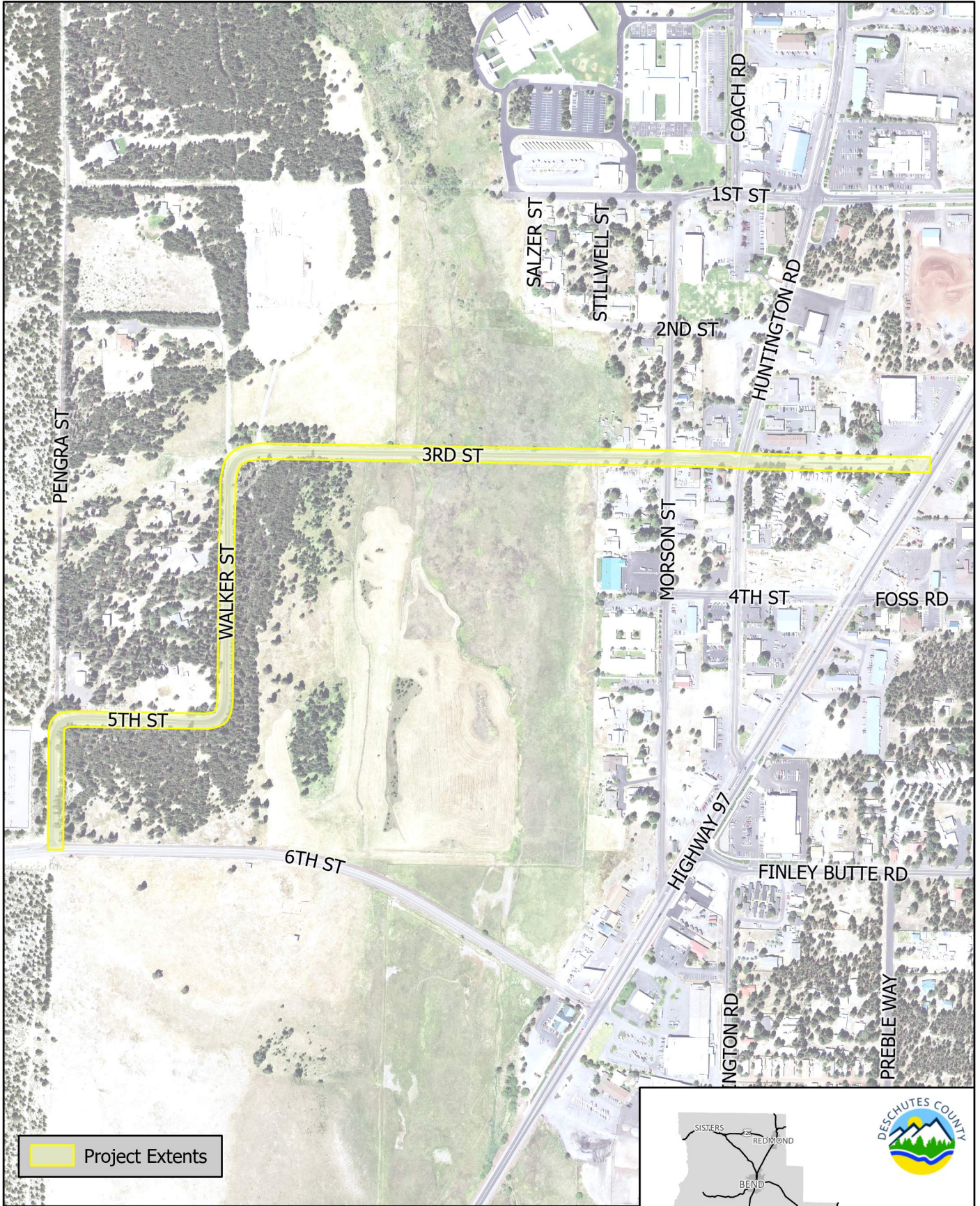
- Asphalt pavement rehabilitation via inlay/overlay
- Isolated full-depth pavement repair
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$580,000	\$580,000
TOTAL	\$580,000	\$580,000

SCHEDULE

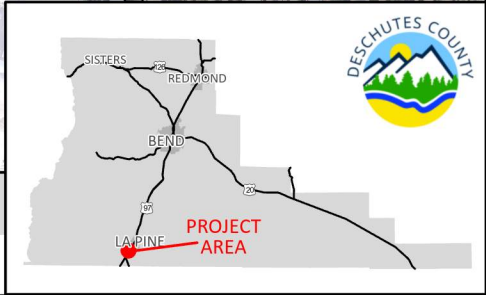
	<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



 Project Extents

2027

PAVEMENT PRESERVATION PROJECT
 3RD ST / WALKER ST / PENGRA ST / 5TH



Paving of 6th Street (La Pine)

6th Street is a rural collector roadway south of La Pine. The roadway provides access to US 97 for rural communities located west of the City of La Pine. 6th Street is exhibiting load- and climate-related pavement distress.



- Project Justification: Pavement Condition Index (PCI) Rating – 84 out of 100
- Functional Classification: Rural Collector
- ADT: 1,372 (2022)

SCOPE OF WORK

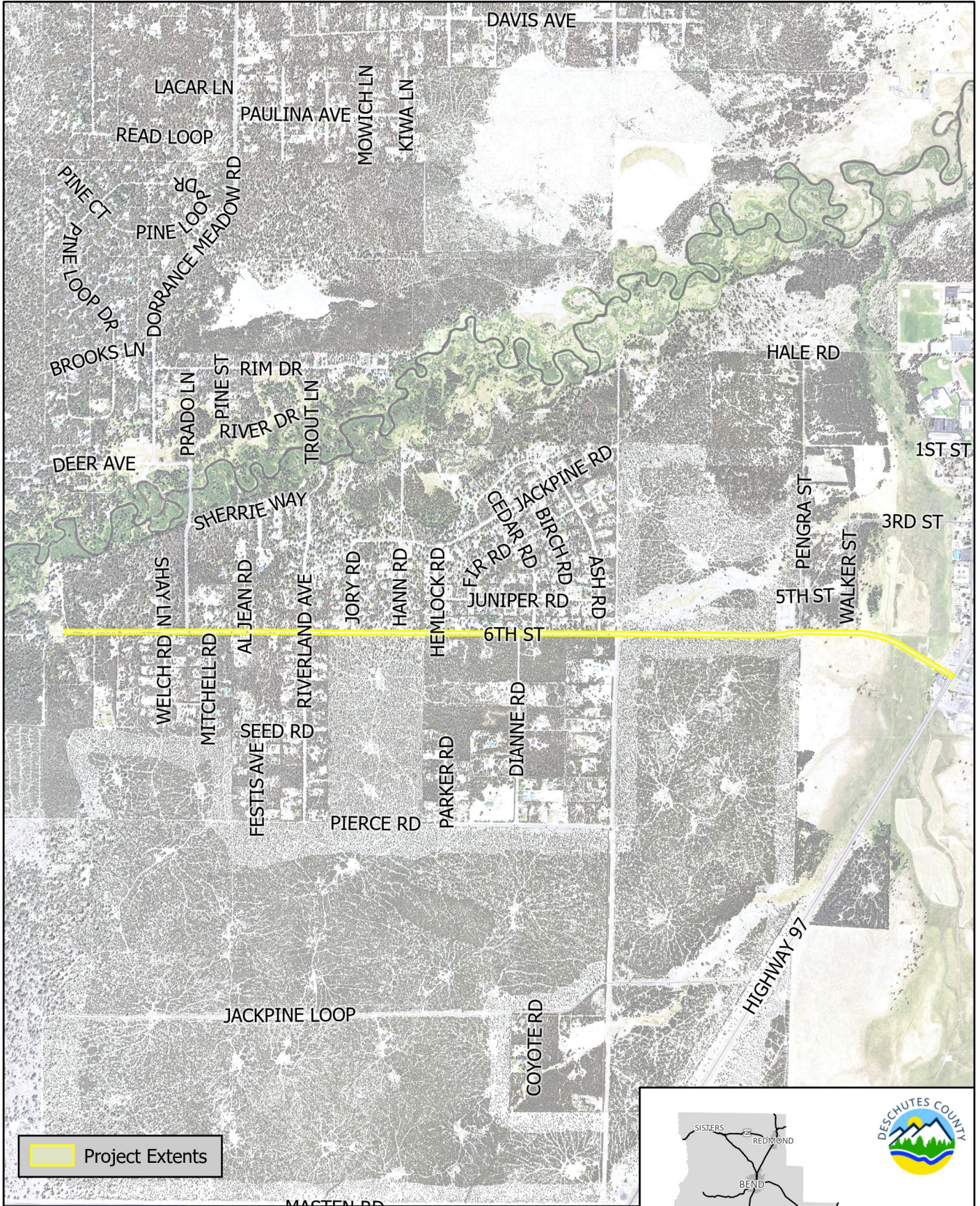
- Asphalt pavement rehabilitation via inlay/overlay
- Isolated full-depth pavement repair
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$1,210,000	\$1,210,000
TOTAL	\$1,210,000	\$1,210,000

SCHEDULE

	<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				

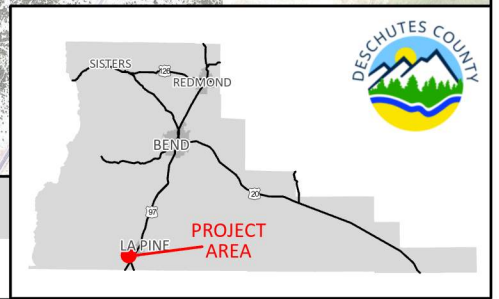


 Project Extents

2028

PAVEMENT PRESERVATION PROJECT

6TH ST (LA PINE)



**Paving of Ward Road:
Stevens Road to Gosney Road**

Ward Road is a rural collector east of Bend which connects rural communities southeast of Bend to City Limits and US 20. The pavement on Ward Road is exhibiting moderate-severity thermal cracking and depressions, resulting in poor ride quality.



- Project Justification: Pavement Condition Index (PCI) Rating – 87 to 91 out of 100
- Functional Classification: Rural Collector
- ADT: 1,896 (2022)

SCOPE OF WORK

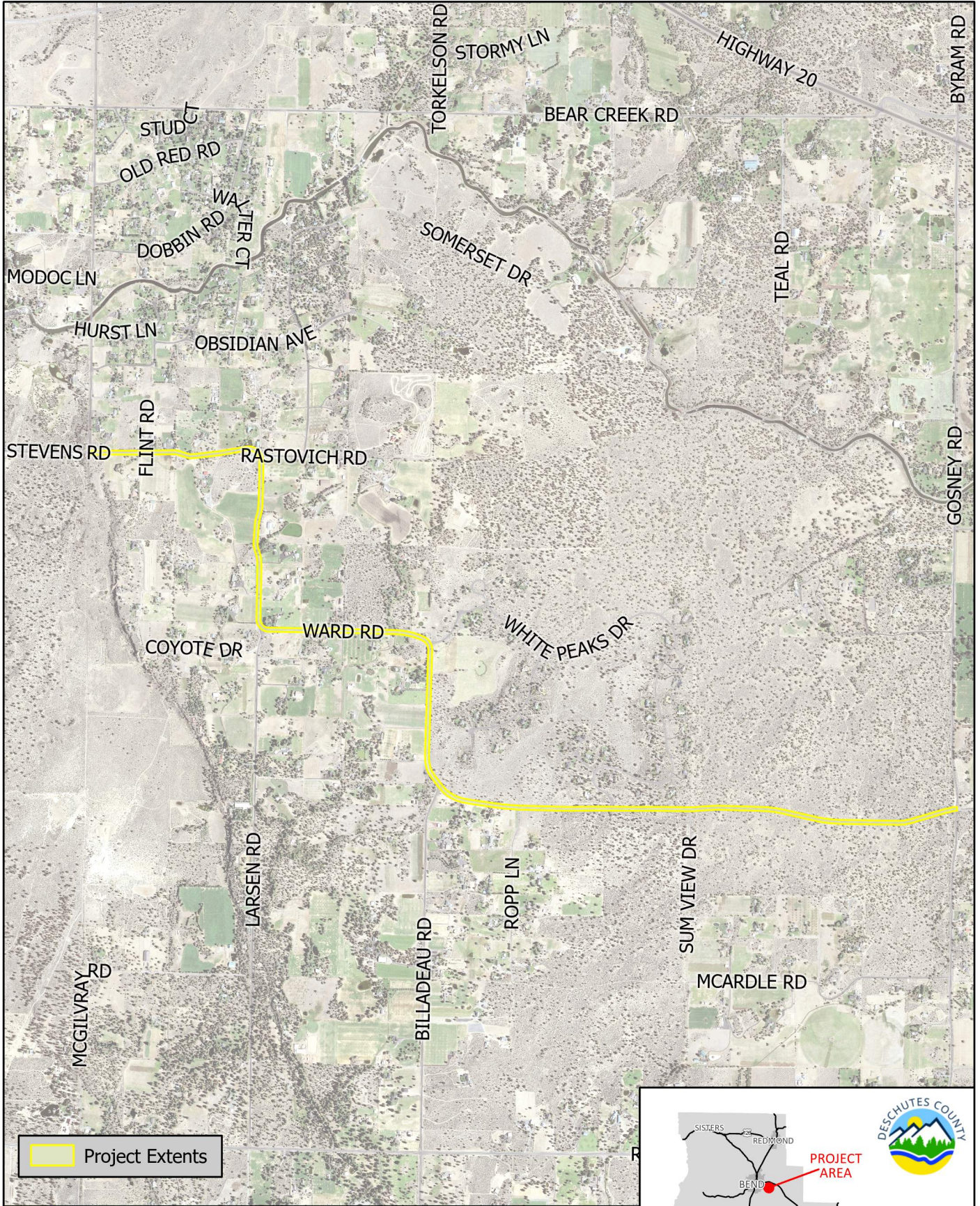
- Rehabilitation of existing pavement via overlay/inlay
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	-	-
RIGHT OF WAY	-	-
CONSTRUCTION	\$1,700,000	\$1,700,000
TOTAL	\$1,700,000	\$1,700,000

SCHEDULE

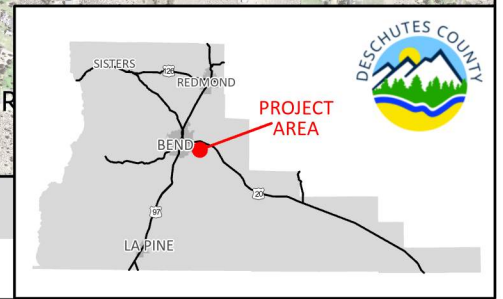
	<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2028

PAVEMENT PRESERVATION PROJECT

WARD RD: STEVENS RD TO GOSNEY RD



Local Road Pavement Preservation

Pavement preservation on local roads generally consists of slurry seals or asphalt concrete pavement overlays on local road networks in individual maintenance zones (Bend, Redmond, Sisters, and La Pine). Annual selection of local roads to receive pavement preservation work is determined based on the area-level pavement conditions contained in the Department’s pavement management system (PMS).



SCOPE OF WORK

- Contracted paving or sealing of local roads

FUNDING

	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
TOTAL	\$200,000	\$200,000	\$200,000	\$200,000	\$200,000	\$1,000,000

Smith Rock Way Bridge #15452
Repalcement

The Smith Rock Way Bridge is a timber bridge located east of Terrebonne that was constructed in 1971. The bridge is structurally deficient and load posted at 30 tons. The bridge and substructure are exhibiting signs of deterioration which warrants replacement.



Smith Rock Way is an east-west arterial roadway linking the communities east of Terrebonne to US 97. The route is frequently used by heavy trucks as a connection to Crook County due to length restrictions on the nearby OR 370.

- Project Justification: Bridge Sufficiency Rating – 76.9 out of 100
- Bridge Name: North Unit Canal (Smith Rock Rd) Bridge #17C02
- Road Name: Smith Rock Way
- Functional Classification: Rural Arterial
- ADT: 929 (2022)

SCOPE OF WORK

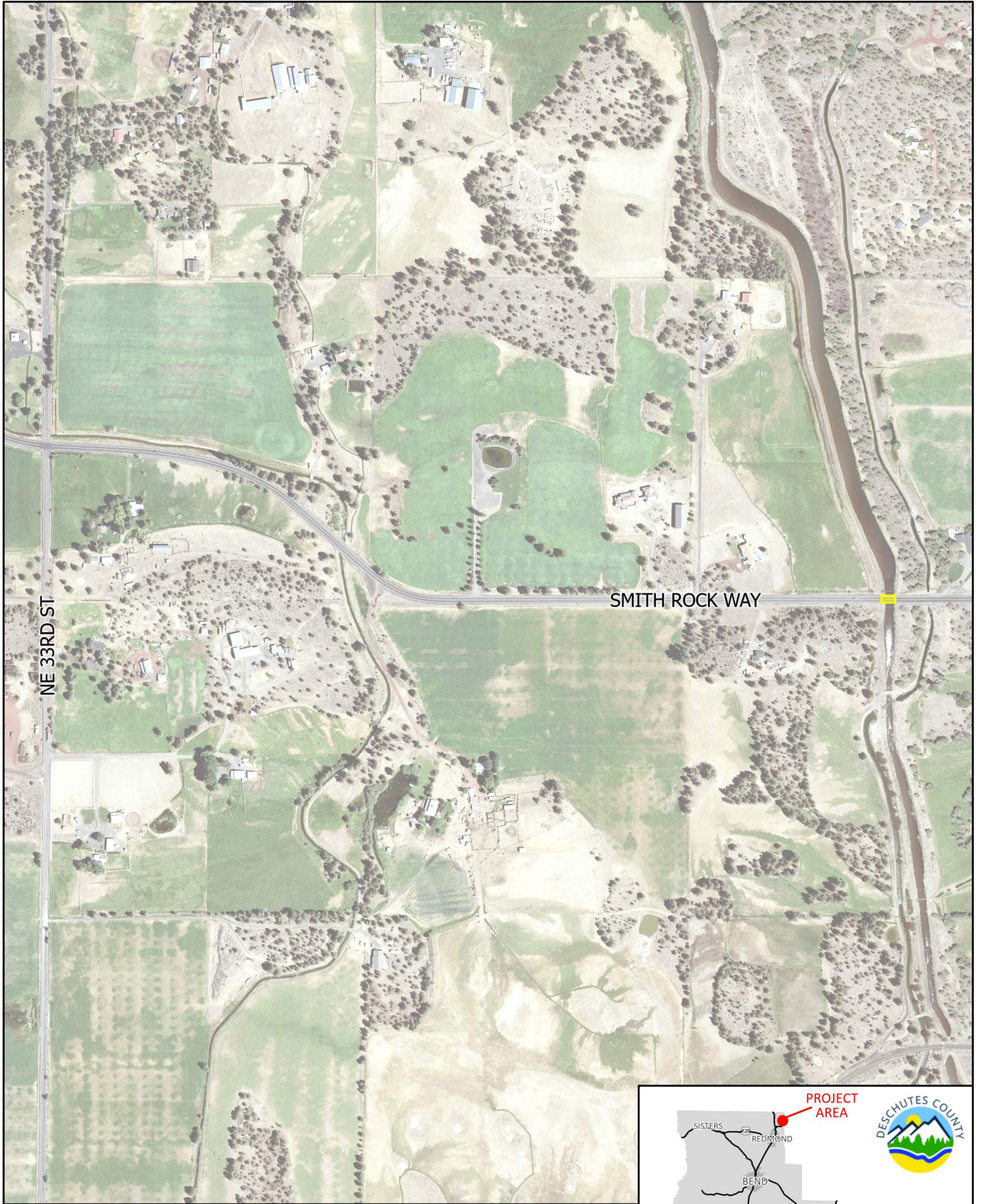
- Removal of the existing structure
- Construction of a single-span, precast, prestressed concrete slab structure
- Bridge approach paving
- Installation of bridge approach guardrail

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$256,699	-	\$256,699
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$1,417,429	\$1,417,429
TOTAL	\$256,699	\$1,417,429	\$1,674,128

SCHEDULE

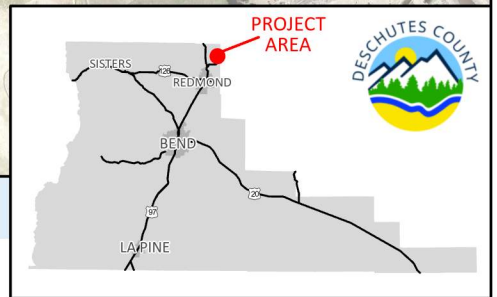
	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2024

BRIDGE CONSTRUCTION PROJECT

SMITH ROCK WAY BRIDGE # 15452 REPLACEMENT



Gribbling Road Bridge #17C30
Repalcement

The Gribbling Road Bridge is a single-lane timber bridge located east of Bend that was constructed in 1958. The bridge is structurally deficient and scour critical. The bridge is load-posted at 5 tons.



Gribbling Road is an important local roadway connecting the communities southeast of Bend to US 20. The route also serves as secondary access to these communities for emergency services. Because of the width and load restriction of the bridge, Deschutes County Rural Fire Protection District #2 will not use the bridge, resulting in longer response times.

- Project Justification: Deschutes County TSP 2010 – 2030 (Low Priority)
 ODOT Local Bridge Program
 Bridge Sufficiency Rating – 17.0 out of 100
- Bridge Name: COID Canal (Gribbling Road) Bridge #17C02
- Road Name: Gribbling Rd
- Functional Classification: Rural Local
- ADT: 147 (2022)

SCOPE OF WORK

- Removal of the existing structure
- Construction of a single-span concrete slab structure
- Bridge approach paving & Installation of bridge approach guardrail

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	ODOT LBP	ODOT LBP	ODOT LBP
PRELIM. ENGINEERING	\$267,106	-	\$267,106
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$704,116	\$704,116
TOTAL	\$267,106	\$704,116	\$971,222

SCHEDULE

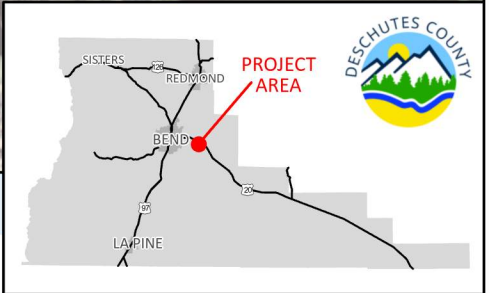
	<i>FY 2023</i>				<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2024

BRIDGE CONSTRUCTION PROJECT

GRIBBLING RD BRIDGE # 17C30 REPLACEMENT (ODOT LBP/SFLP)



Wilcox Avenue Bridge
(#02171-03 & -04) Removal

Two timber bridges exist on NE Wilcox Avenue approximately .4 miles west of the Crook County line, which formerly served as overcrossings for the Lone Pine Flume. The Lone Pine Flume has since been piped and the bridges are now obsolete. Built in 1969, both bridges are experiencing rot, spalling on concrete abutments and checking throughout the timber structures.



NE Wilcox Avenue is an east-west collector located east of Terrebonne that runs parallel to Smith Rock Way from NE 1st St, and after approximately 3 miles crosses into Crook County where it connects to Smith Rock Way. This roadway serves as the primary access to Smith Rock State Park for recreational users.

- Bridge Name: Wilcox Ave Bridge # 02171-03 Wilcox Ave Bridge # 02171-04
- Bridge Sufficiency Rating: 90.5 out of 100 90.5 out of 100
- Road Name: NE Wilcox Ave NE Wilcox Ave
- Functional Classification: Rural Collector Rural Collector
- ADT: 1,346 (2022) 1,346 (2022)

SCOPE OF WORK

- Removal of existing timber structures
- Construction of a new paved roadway section
- Minor improvements – Striping and Delineation

FUNDING

	<i>FY 2024</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$60,000	\$60,000
RIGHT OF WAY	-	-
CONSTRUCTION	\$100,000	\$100,000
TOTAL	\$160,000	\$160,000

SCHEDULE

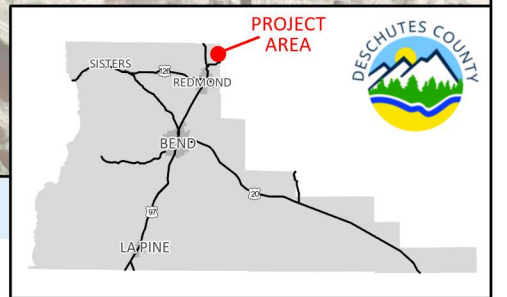
	<i>FY 2024</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING				
RIGHT OF WAY				
CONSTRUCTION				



2024

BRIDGE CONSTRUCTION PROJECT

WILCOX AVE BRIDGE # 2171-03 & 04 REMOVAL



Hamehook Road Bridge #17C32 Replacement

The Hamehook Road Bridge is a timber bridge located northeast of Bend that was constructed in 1977. As development and population increase to the north and east of Bend, Hamehook Road continues to be an important north-south route for road users bypassing Bend for access to and from US 97.



The timber structure is experiencing checking and cracking throughout the deck and girders. The existing concrete piers, abutments and footings are delaminating and cracking, and the metal bridge railing is substandard and in need of replacement.

- Project Justification: Bridge Sufficiency Rating – 73.9 out of 100
- Bridge Name: North Unit Main Canal (Hamehook Road) Bridge #17C32
- Road Name: Hamehook Road
- Functional Classification: Rural Collector
- ADT: 3,564 (2022)

SCOPE OF WORK

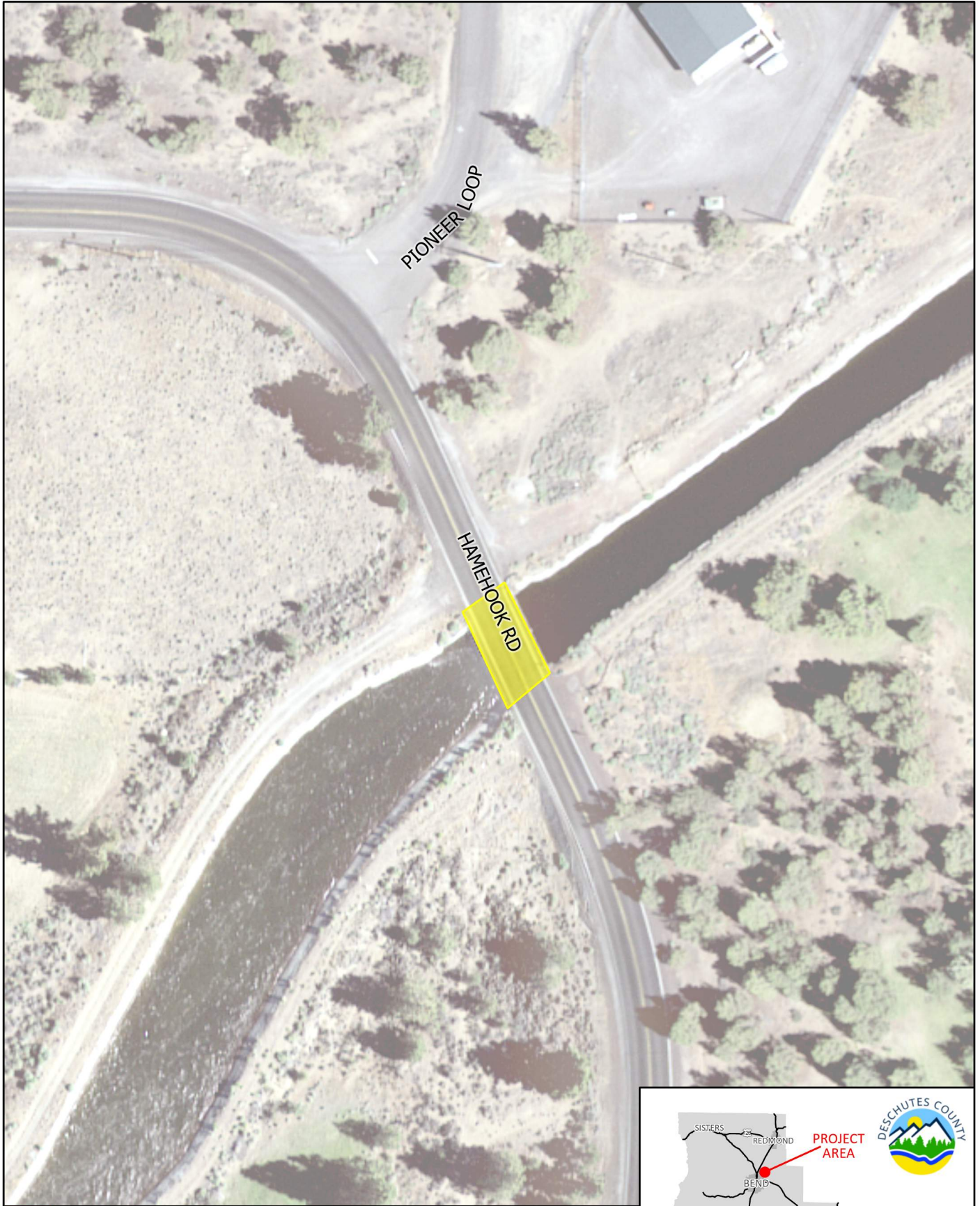
- Construction of a temporary detour bridge
- Removal of the existing structure
- Construction of a single-span, precast, prestressed concrete slab structure
- Bridge approach paving
- Installation of bridge approach guardrail

FUNDING

	<i>FY 2023</i>	<i>FY 2024</i>	<i>FY 2025</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$40,000	\$230,000	-	\$270,000
RIGHT OF WAY	-	\$65,000	-	\$65,000
CONSTRUCTION	-	\$300,000	\$1,200,000	\$1,500,000
TOTAL	\$40,000	\$595,000	\$1,200,000	\$1,835,000

SCHEDULE

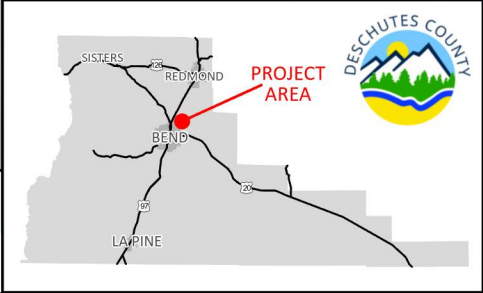
	<i>FY 2023</i>				<i>FY 2024</i>				<i>FY 2025</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2025

BRIDGE CONSTRUCTION PROJECT

HAMEHOOK RD BRIDGE # 17C32 REPLACEMENT



Local Access Road Bridges

Various Local Access Road Bridges exist in Deschutes County. Some are in disrepair and pose safety hazards to the travelling public and Emergency & Medical Services, and as such, are in need of replacement. Deschutes County Road Department is obligating funds for the replacement of these bridges to maintain safe and continuous access to rural communities constrained by waterways or other topographical features.



SCOPE OF WORK

- Evaluation of the existing bridge structures
- Maintenance or Removal & Replacement of bridge structures, as needed
- Paving of approaches
- Installation of approach guardrail

FUNDING

	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$50,000	\$50,000	\$50,000	\$50,000	\$200,000
RIGHT OF WAY	-	-	-	-	-
CONSTRUCTION	\$100,000	\$200,000	\$200,000	\$200,000	\$700,000
TOTAL	\$150,000	\$250,000	\$250,000	\$250,000	\$900,000

SCHEDULE

	<i>FY 2024</i>				<i>FY 2025</i>				<i>FY 2026</i>				<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING	■	■			■	■			■	■			■	■		
RIGHT OF WAY																
CONSTRUCTION			■	■			■	■			■	■			■	■

South Century Drive Bridge #16181 Rehabilitation

The South Century Drive Bridge #16181 over the Burlington Northern – Santa Fe (BNSF) railroad is a 3-span prestressed concrete bridge located east of Sunriver that was constructed in 1976. South Century Drive is an east-west arterial located near Sunriver that connects US 97 to the Cascade Lakes Highway. The segment from US 97 to Sunriver is a primary access route for recreational users and residents of Sunriver to/from US 97.



The bridge is showing significant signs of efflorescence, cracking and spalling throughout the structure, and the concrete railing and is suspected to have been constructed with poor quality concrete materials.

- Project Justification: Bridge Sufficiency Rating – 77.2 out of 100
- Bridge Name: South Century Dr over BNRR Bridge #16181
- Road Name: South Century Drive
- Functional Classification: Rural Arterial
- ADT: 8,435 (2022)

SCOPE OF WORK

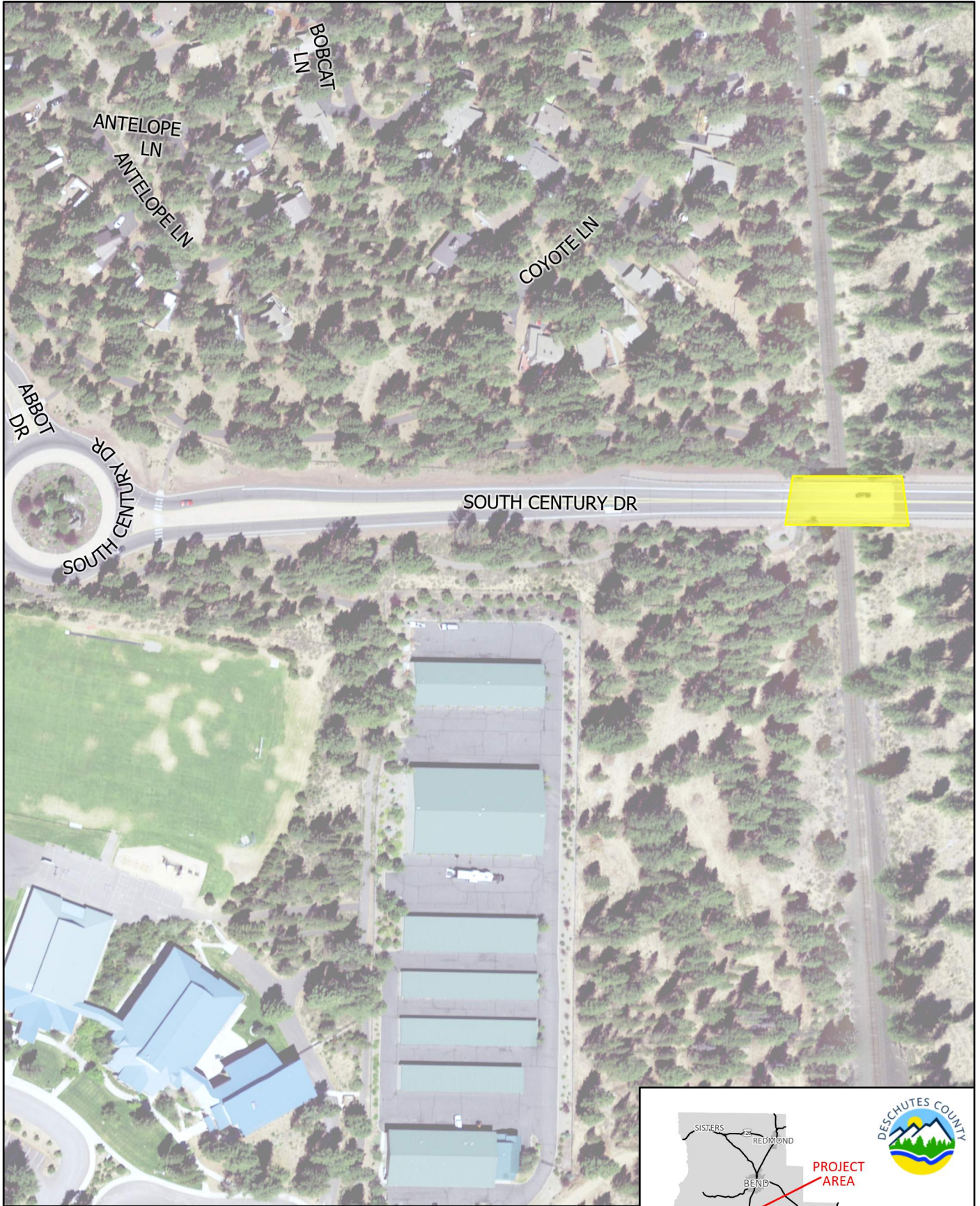
- Repair reinforced concrete bridge components
- Replace concrete bridge rail
- Replace bridge deck surfacing and install waterproofing membrane
- Upgrade bridge approach guardrail

FUNDING

	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>TOTAL</i>
FUNDING SOURCE	ODOT LBP	ODOT LBP	ODOT LBP	ODOT LBP
PRELIM. ENGINEERING	\$100,000	\$410,000	-	\$510,000
RIGHT OF WAY	-	-	-	-
CONSTRUCTION	-	\$600,000	\$1,000,000	\$1,600,000
TOTAL	\$100,000	\$1,010,000	\$1,000,000	\$2,110,000

SCHEDULE

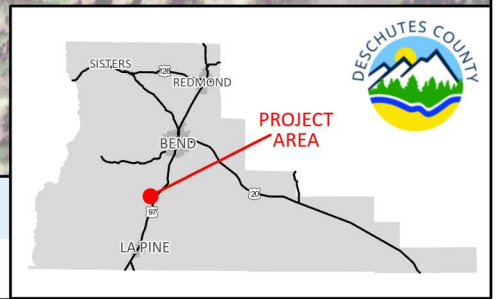
	<i>FY 2026</i>				<i>FY 2027</i>				<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2027

BRIDGE CONSTRUCTION PROJECT

S CENTURY DR BRIDGE # 16181 REHABILITATION (ODOT LBP/SFLP)



Burgess Road Bridge #09C783
Replacement

The Burgess Road Bridge is a single-span steel girder bridge located west of La Pine that was constructed in 1962. Burgess Road carries recreational traffic from US 97 and La Pine to the Deschutes National Forest and South Century Drive, and also serves several rural residents in the area.



The concrete deck is exhibiting cracking on the wearing surface and soffit, and the reinforced concrete abutments are spalling and cracking. The existing footings are exposed, and the metal bridge railing is substandard and in need of replacement.

- Project Justification: Bridge Sufficiency Rating – 51.8 out of 100
- Bridge Name: Deschutes River, Burgess Rd Bridge #09C783
- Road Name: Burgess Road
- Functional Classification: Forest Highway
- ADT: 136 (2022)

SCOPE OF WORK

- Removal of the existing structure
- Construction of a single-span, precast, prestressed concrete slab structure
- Bridge approach paving
- Installation of bridge approach guardrail

FUNDING

	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$100,000	\$315,000		\$415,000
RIGHT OF WAY	-	-	-	-
CONSTRUCTION	-	\$250,000	\$1,453,000	\$1,703,000
TOTAL	\$100,000	\$565,000	\$1,453,000	\$2,118,000

SCHEDULE

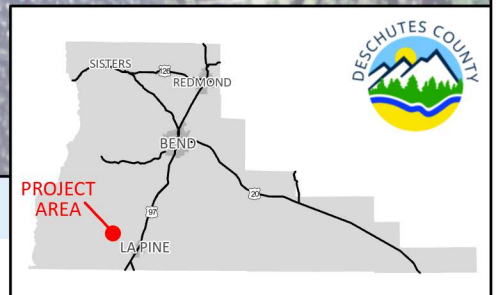
	<i>FY 2025</i>				<i>FY 2026</i>				<i>FY 2027</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING												
RIGHT OF WAY												
CONSTRUCTION												



2028

BRIDGE CONSTRUCTION PROJECT

BURGESS RD BRIDGE # 09C783 REPLACEMENT



**Spring River Road (Harper) Bridge
#17923 Rehabilitation**

The Spring River Road (Harper) Bridge #17923 over the Deschutes River is a 3-span concrete structure constructed in 1994. Spring River Road is an arterial roadway which carries recreational traffic to the Deschutes National Forest and links US 97 with communities west of Sunriver.



The reinforced concrete girders are spalling and water intrusion is occurring from the slab joints. The steel piles within the river channel are also exhibiting corrosion, and the concrete bridge railing is showing signs of deterioration.

- Project Justification: Bridge Sufficiency Rating – 59.9 out of 100
- Bridge Name: Deschutes River, Spring River Rd Bridge #17923
- Road Name: Spring River Road
- Functional Classification: Rural Arterial
- ADT: 5,599 (2022)

SCOPE OF WORK

- Repair reinforced concrete bridge components
- Replace concrete bridge rail
- Replace piles within river channel
- Upgrade bridge approach guardrail

FUNDING

	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
PRELIM. ENGINEERING	\$100,000	\$50,000	\$150,000
RIGHT OF WAY	-	-	-
CONSTRUCTION	-	\$380,000	\$380,000
TOTAL	\$100,000	\$430,000	\$530,000

SCHEDULE

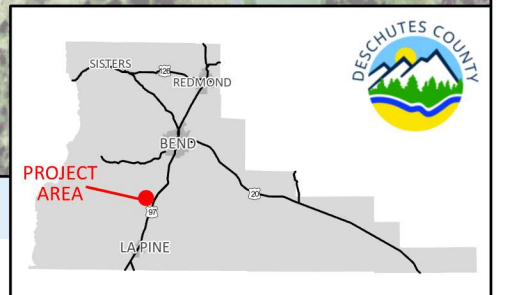
	<i>FY 2027</i>				<i>FY 2028</i>			
	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>	<i>QTR 1</i>	<i>QTR 2</i>	<i>QTR 3</i>	<i>QTR 4</i>
PRELIM. ENGINEERING								
RIGHT OF WAY								
CONSTRUCTION								



2028

BRIDGE CONSTRUCTION PROJECT

SPRING RIVER RD (HARPER) BRIDGE #17923 REHABILITATION



Signage Improvements

Annual signage improvements include systemic safety improvements on County road intersections and curves. Signage work includes installing new traffic control devices or upgrading existing traffic control devices in accordance with current State and Federal standards. Additionally, localized traffic control device improvements are sometimes warranted to address specific operational or safety needs.



SCOPE OF WORK

- Contracted installation of regulatory signs, warning signs, and other traffic control devices

FUNDING

	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
TOTAL	\$150,000	\$150,000	\$100,000	\$100,000	\$100,000	\$600,000

Guardrail Improvements

Annual guardrail improvements include upgrading existing guardrail systems to current State and Federal standards and installing new guardrail systems at locations where warranted.



SCOPE OF WORK

- Contracted installation of guardrail systems

FUNDING

	<i>FY 2024</i>	<i>FY 2025</i>	<i>FY 2026</i>	<i>FY 2027</i>	<i>FY 2028</i>	<i>TOTAL</i>
FUNDING SOURCE	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.	DESCHUTES CO.
TOTAL	\$150,000	\$150,000	\$100,000	\$100,000	\$100,000	\$600,000